

**ADDENDUM NO. ONE (1)**

**REHABILITATE AIRFIELD SIGNAGE AND LIGHTING**

**ROANOKE-BLACKSBURG REGIONAL AIRPORT  
ROANOKE, VIRGINIA**

**AIP PROJECT NO. 3-51-0045-062-2019 (DESIGN)  
AIP PROJECT NO. 3-51-0045-PENDING (CONSTRUCTION)  
DELTA PROJECT NO. 19013**

**JUNE 3, 2020**

From: Delta Airport Consultants, Inc.  
270 Polo Parkway  
Midlothian, Virginia 23113

To: **All Plan Holders of Record**



This Addendum is hereby made a part of the contract documents and specifications of the above referenced project. All other requirements of the original specification shall remain in effect in their respective order. **Acknowledge receipt of this addendum by inserting its number and date in the proposal form.**

**INVITATION TO BID**

1. **REPLACE** the "INVITATION TO BID" with the attached "INVITATION TO BID" of Addendum No. One (1). Please note the Bid due date has been shifted to 11:00 A.M. on Friday, June 12, 2020.

**BID FORMS**

2. **REPLACE** the "BID FORMS" with the attached "REVISED BID FORMS" of Addendum No. One (1).

## ADDENDUM NO. ONE (1)

### SPECIFICATIONS

3. **REVISE** the first paragraph of Section 30-12 to read as follows:

“The Contractor will be issued two Notices to Proceed for the work under this Contract. The first Notice to Proceed for Phase 1 Administrative Services will be issued upon the return of the executed Contract to Contractor. During Phase 1, Contractor will attend a preconstruction conference, prepare, submit and have approved work and phasing schedules, safety plans, color samples, other required submittals, etc.; arrange for its employees and/or subcontractors to be background checked, trained and badged for access to and work within the airport secured areas; and order supplies and other equipment needed for the project. All work preliminary to the actual start of labor at the Airport must commence within ten (10) calendar days and completed within One-Hundred Twenty (120) calendar days of the effective date of the first Notice to Proceed. Review time by the Owner and/or Engineer during which Contractor can undertake none of the administrative tasks shall not be counted as part of this time period.”

4. **REVISE** the second paragraph of Section 30-12 to read as follows:

“The second Notice to Proceed will be for Phase 2 – Performance of the Work, which will be issued by Owner at the expiration of the Phase I time period. All Phase 2 Work on site shall commence within ten (10) calendar days and shall be completed within Three Hundred Thirty (330) calendar days of the effective date of the Phase 2 Notice to Proceed, including all inspections and testing procedures required by these contract documents.”

5. **REVISE** the first paragraph of Note 57 under “Completion of Construction” in GP 150-23 to read as follows:

“(Incidental to Pay Item L-109) Provide a minimum of (12) twelve hours of training for the owner’s maintenance personnel (6 attendees minimum). Training sessions shall be broken up into three (3) four-hour sessions. Training dates and times shall be scheduled by the owner at a location on airport property for convenience of attendees. Provide all required visual aids and projection equipment, unless directed otherwise by owner. Submit a training session agenda with topics, estimated topic duration and learning objectives for approval and distribution. Training shall be performed by contractor and/or manufacturer authorized representatives unless directed otherwise by the owner, the sessions shall include:”

6. **ADD** the attached Item P-605 “Joint Sealants for Pavements” of Addendum No. One (1).

7. **ADD** the following pay item to Item L-119-5.1:

Item L-119-5.1            Install Solar Airport Obstruction Light (LED) – per each

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- 8. ADD** the following pay items to Item L-125-5.1:

Item L-125-5.1	L-858(L) Airfield Guidance Sign (1-2 Char.) On New Foundation In Existing Pavement - per each
Item L-125-5.1	L-858(L) Airfield Guidance Sign (3-4 Char.) On New Foundation In Existing Pavement - per each
Item L-125-5.1	L-858(L) Airfield Guidance Sign (5-6 Char.) On New Foundation In Existing Pavement - per each
Item L-125-5.1	L-858(L) Airfield Guidance Sign (7-8 Char.) On New Foundation In Existing Pavement - per each

- 9. REVISE** the first paragraph of Section “3. TERM” on Page C-1 of the Contract to read as follows:

“Contractor agrees that time is of the essence for completion of this Contract. All preliminary and administrative work shall be completed within One Hundred Twenty (120) consecutive calendar days after the effective date of the written Phase 1 Administrative Services Notice to Proceed. All Work at the Airport, including the replacement of all indicated airfield lights and signs, vault equipment, all associated services, and final clean-up, shall be completed and final acceptance issued within Three Hundred Thirty (330) consecutive calendar days after the effective date of the written Phase 2 Performance of Work Notice to Proceed. Contractor shall notify the Owner in writing received at least 48 hours in advance of the date it desires to begin the Work at the site. The work, once begun in any area, must continue uninterrupted until completion.”

**PLANS**

**SHEET 2 OF 68**

- 10. REPLACE** the “SUMMARY OF QUANTITIES” table with the attached “SUMMARY OF QUANTITIES” table shown in the attached Exhibit 2-1 of Addendum No. One (1).

**SHEET 10 OF 68**

- 11. REVISE** Note No. 1 of the “GENERAL” Phasing Notes to read as follows:

“1. THE CONTRACTOR SHALL HAVE 330 CALENDAR DAYS (CD) TO COMPLETE THE PROJECT.”

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**SHEETS 15 THROUGH 33 OF 68**

- 12. REPLACE** the “LEGEND” portion of each sheet with the attached “LEGEND” shown in the attached Exhibit 15-1 of Addendum No. One (1).

**SHEET 16 OF 68**

- 13. REVISE** the leader note on the leftmost portion of the sheet as shown below to remove the reference to “Bid Additive 1 Only”:

“INSTALL NEW LED OBSTRUCTION LIGHT ON EXISTING RPU STATION”

**SHEETS 33 THROUGH 43 OF 68**

- 14. REPLACE** the “LEGEND” portion of each sheet with the attached “LEGEND” shown in the attached Exhibit 33-1 of Addendum No. One (1).

**SHEET 47 OF 68**

- 15. REPLACE** the “PULLCAN DETAIL” with the attached “PULLCAN DETAIL” shown in the attached Exhibit 47-1 of Addendum No. One (1)

**SHEET 51 OF 68**

- 16. REVISE** Note No. 5 on the “SHALLOW BASE CAN IN-PAVEMENT LIGHT WIRING ARRANGEMENT” detail to read as follows:

“5. ISOLATION TRANSFORMER PULLCANS CONNECTED WITH STANDARD #8, 5KV CABLE IN 2" CONDUIT AT 24" BELOW GRADE, AND COUNTERPOISE ABOVE.”

- 17. REVISE** Note No. 5 on the “EXISTING SHALLOW CAN IN-PAVEMENT LIGHT” detail to read as follows:

“5. REMOVAL AND REPLACEMENT OF BACKER ROD AND SEALANT IN EXISTING SAW KERFS SHALL BE PAID PER LINEAR FOOT UNDER P-605 “JOINT SEALING FILLER”.”

- 18. REPLACE** the “SAW KERF WIREWAY” detail with the attached “SAW KERF WIREWAY” detail shown in the attached Exhibit 51-1 of Addendum No. One (1).

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### SHEET 52 OF 68

19. **REVISE** Note No. 9 on the “LED AIRFIELD GUIDANCE SIGN DETAIL” to read as follows:

“9. CONCRETE SURFACE SHALL RECEIVE A BROOM FINISH.”

### SHEET 52A OF 68

20. **ADD** Sheet 52A.
21. **ADD** the “LED AIRFIELD GUIDANCE SIGN DETAIL (OPTIONAL PRECAST PAD)” as shown in the attached Exhibit 52A-1 of Addendum No. One (1).

### SHEET 53 OF 68

22. **REVISE** the leader note to the spare conduit on the “LED AIRFIELD GUIDANCE SIGN DETAIL – IN PAVEMENT” to read as follows:

“SPARE 2" PVC CONDUIT, CAP AT 4" BEYOND CONCRETE ENCASEMENT”

23. **REVISE** Note No. 9 on the “LED AIRFIELD GUIDANCE SIGN DETAIL – IN PAVEMENT” to read as follows:

“9. CONCRETE SURFACE SHALL RECEIVE A BROOM FINISH.”

### SHEET 57 OF 68

24. **REPLACE** the “TEMPORARY SIGN PANEL INFORMATION” for Signs # 132 and 133 with the attached “TEMPORARY SIGN PANEL INFORMATION” as shown in the attached Exhibit 57-1 of Addendum No. One (1).

### SHEET 52 OF 68

25. **REVISE** Note No. 5 on the “PRIMARY LED WIND CONE DETAIL (L-807) INTERNALLY LIGHTED – VOLTAGE DRIVEN” to read as follows:

“5. CONCRETE SURFACE SHALL RECEIVE A BROOM FINISH.”

26. **REVISE** Note No. 4 on the “SUPPLEMENTAL LED WIND CONE DETAIL (L-806) INTERNALLY LIGHTED” to read as follows:

“4. CONCRETE SURFACE SHALL RECEIVE A BROOM FINISH.”

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### **SHEET 59 OF 68**

- 27. ADD** the “SOLAR OBSTRUCTION LIGHT DETAIL “as shown in the attached Exhibit 59-1 of Addendum No. One (1).

### **SHEET 62 OF 68**

- 28. REVISE** Keynote N2 to read as follows:

“NEW - PREPARED LOCATION AND DEDICATED SPACE FOR STACKED FUTURE CCR'S. SEE SHEET 64 - ELECTRICAL SINGLE LINE RISERS AND SHEET 65 - AIRFIELD CIRCUIT SEGMENTATION DIAGRAM FOR ADDITIONAL INFORMATION”

### **SHEET 63 OF 68**

- 29. REVISE** Keynote N7 to read as follows:

“36"W x 72"T x 18"D 5KV BREAKOUT PATCH PANEL ENCLOSURE. OPEN BOTTOM, NEMA 12, FREE STANDING, LOCKABLE, WITH 3 POINT LATCHING DOOR AND BACK MOUNTING PLATE. PROVIDE SIGNAGE READING”

### **SHEET 65 OF 68**

- 30. REPLACE** the “5KV AFL CIRCUIT PHASING AND LABELING” table with the attached “5KV AFL CIRCUIT PHASING AND LABELING” table as shown in the attached Exhibit 65-1 of Addendum No. One (1).

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**ATTACHMENTS:**

1. Questions and Answers
2. Revised Invitation to Bid
3. Item P-605 Joint Sealants for Pavements
4. Exhibit 2-1
5. Exhibit 15-1
6. Exhibit 33-1
7. Exhibit 47-1
8. Exhibit 51-1
9. Exhibit 52A-1
10. Exhibit 57-1
11. Exhibit 59-1
12. Exhibit 65-1
13. Revised Bid Forms

**END OF ADDENDUM NO. ONE (1)**

**ATTACHMENT NO. ONE (1)**

**REHABILITATE AIRFIELD SIGNAGE AND LIGHTING**

**ROANOKE-BLACKSBURG REGIONAL AIRPORT  
ROANOKE, VIRGINIA**

**QUESTIONS AND ANSWERS**

1. QUESTION: Are bidders required to submit evidence of competency and financial responsibility with their bid submission?

ANSWER: Yes. Refer to Section 20 for bid submission requirements.
2. QUESTION: Section 20-03 and Section C-105-2 – Mobilization Limit, both state that the bid item for Mobilization is limited to 10% of the total project cost. Will this be enforced?

ANSWER: Yes.
3. QUESTION: What happens if a bidder submits a bid with a Mobilization cost greater than 10% of the total project cost?

ANSWER: If a bidder submits a bid where Mobilization is greater than 10% of the total project cost, their bid may be deemed unresponsive by the Owner and rejected.
4. QUESTION: Do any other bid items have a similar percentage or monetary limitation?

ANSWER: No. However, the overall bid is still subject to the provisions of Section 20-09d relating to unbalanced bids.
5. QUESTION: Do you anticipate extending the bid due date?

ANSWER: Yes. The bid opening has been extended to 11:00 a.m. on Friday, June 12<sup>th</sup>, 2020. See the revised Invitation to Bid of Addendum No. One (1).
6. QUESTION: Will the contract time change?

ANSWER: The overall Contract Time has been revised to 330 Calendar Days.



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7.     **QUESTION:**           What additional details are you willing to provide, if any, beyond what is stated in bid documents concerning how you will identify the winning bid?
- ANSWER:**             Award is to be made as described in the bid documents.
8.     **QUESTION:**           Was this bid posted to the nationwide free bid notification website at [www.mygovwatch.com](http://www.mygovwatch.com)?
- ANSWER:**             No.
9.     **QUESTION:**           Other than your own website, where was this bid posted?
- ANSWER:**             Reference is directed to the “Invitation to Bid”.
10.    **QUESTION:**           Where are the endwalls to be installed?
- ANSWER:**             One endwall is to be installed as shown on Sheet 31 of the Plans. The second endwall is to be field located to drain water from the conduit system as required during construction.
11.    **QUESTION:**           Is there any hazmat information available on the existing regulators to be removed?
- ANSWER:**             There are Seven (7) Oil-Filled Existing L-828 #31060 Crouse-Hinds constant current regulators.
12.    **QUESTION:**           Are utilities available in the staging area shown on the plans?
- ANSWER:**             No.
13.    **QUESTION:**           Where is boring to be performed?
- ANSWER:**             Boring, if required due to issues with the existing conduits or ducts, will be located in the field.

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14.    **QUESTION:**            On mulching and seeding, was there any anticipated work beyond that around the signs and trenches, e.g. staging area? 10k sq yd?
- ANSWER:**                Seeding and mulching shall be performed on all disturbed areas, including areas around sign pads. The bid quantity of 10,000 square yards is an estimate of the amount required. Payment will be based on field measurements of the actual work completed.
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15.    **QUESTION:**            Will badging be required of all Contractor’s employees and sub-contractor’s employees? What is the airport’s procedure and cost for badging?
- ANSWER:**                Reference is directed to the Security section of the General Notes and Special Provisions of the specifications.
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16.    **QUESTION:**            Specification GP-150, Section 56 notes that the Contractor shall be reimbursed for actual cost of SP-1 Non-AIP Insurance, up to the allowance amount. Where is Non-AIP insurance addressed in the specifications and where is the allowance to be included in the bid?
- ANSWER:**                Section 150-05 “Non-AIP Insurance” addresses Non-AIP insurance. Non-AIP Insurance shall be included under “SP-1 Non-AIP Insurance Allowance” on the Proposal.
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17.    **QUESTION:**            Specification GP-150, Section 57 notes “Provide a minimum of 12 hours of training, broken up into (4) four-hour sessions. – Please clarify.
- ANSWER:**                Refer to the revisions to GP-150-23 included in Addendum No. One (1).
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18.    **QUESTION:**            Drawing 16 shows “Install new LED obstruction light on existing RPU station (Bid Additive 1 Only)”. The bid documents do not include a Bid Additive 1.
- ANSWER:**                The note has been revised. Reference is directed to Exhibit 16-1 of Addendum No. One (1).

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19. QUESTION: Drawing 22 shows a solar powered obstruction light that is noted “Remove existing fixture and install a new LED obstruction light”. Is this included in Bid Item 36-REPLACE OBSTRUCTION LIGHT FIXTURE (LED) IN PLACE? Please provide a specification for the solar obstruction light and power supply.

ANSWER: Reference is directed to Exhibit 59-1 of Addendum No. One (1).

20. QUESTION: Bid Item 34 – NON-ENCASED ELECTRICAL DRAINAGE CONDUIT, 1 WAY 2” PVC CONDUIT – 500 LF. Where is this conduit shown on the drawings? We could only locate about 70 LF on drawing 31, which shows coring into an existing light base, in a paved shoulder (NO SEPARATE PAY ITEM) and installing a 2” PVC drain pipe under an existing gravel service road and repairing the gravel service road (NO SEPARATE PAY ITEM) and also repairing the paved shoulder (INCIDENTAL TO DRAINAGE CONDUIT)

ANSWER: The remainder of the drainage conduit is to be field located as required to drain water from the conduit system as determined during construction.

21. QUESTION: Drawing 47 PULLCAN DETAIL shows a 2” PVC pipe (min. 1% slope) to sump (see note 2). There are no details showing a sump and the drainpipe is not shown on the drawings, except for the one noted previously. Note 2 addresses bonding direct buried bare wire, not sumps.

ANSWER: Reference is directed to Exhibit 47-1 of Addendum No. One (1).

22. QUESTION: Bid Item 35 - ELECTRICAL JUNCTION STRUCTURE – L-867 PULLCAN – 30 each. The LEGEND on the drawings show a L-867B ELECTRICAL PULLCAN – no electrical junction structures shown. Are these the same item? We could only locate 16 L-867B electrical pullcans on the plans – where are the other 14 shown?

ANSWER: The Bid Item and Legend are the same item. The additional pullcans included in the estimate are to be field located, as required, during construction.

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23. QUESTION: Bid Item 46 - L-868B. 12"CLASS 1A LIGHT BASE – 5 each. Where are these shown on the drawings?
- ANSWER: It is anticipated that additional damaged light bases may be identified for replacement during construction.
24. QUESTION: Drawing 52 LED AIRFIELD GUIDANCE SIGN DETAIL indicates that the transformer base, for the sign, is to be installed on the opposite end of the sign from the pavement? Is this correct?
- ANSWER: Yes.
25. QUESTION: Drawing 53 LED AIRFIELD GUIDANCE SIGN DETAIL shows a spare 4" PVC conduit, cap 24" beyond foundation. Is this correct?
- ANSWER: The note has been revised. Reference is directed to the revisions on Sheet 53 noted in Addendum No. One (1).
26. QUESTION: The bid documents do not include any demolition drawings – are the new signs going in the same place as the existing ones?
- ANSWER: The signs are generally located in the same location with any known exceptions noted in the plans.
27. QUESTION: There are approximately 15 new signs that will be installed in existing paved areas. Drawing 53 shows a detail of the new signs in paved areas, however the signs are not identified on the drawings (as being in a paved area). These signs should be identified on the drawings and separate bid item(s) for L-858(L) AIRFIELD GUIDANCE SIGN (X-X CHAR) ON NEW FOUNDATION IN EXISTING PAVEMENT added to the bid items.
- ANSWER: A separate pay items has been added for the referenced signs. Reference is directed to the Specification revisions and Revised Bid Forms included in Addendum No. One (1).

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28. QUESTION: Drawings 54, 55, 56 & 57 show all of the new signs to be Style 3 (5 step 2.8A-6.6A). Drawing 65 shows the CCRs to be either 3 step (4.8A-6.6A) or 5 step (2.8A-6.6A). The signs should match the circuit that they are connected to.

ANSWER: As noted on Sheets 63 and 64 all new CCR's shall be 5-Step.

29. QUESTION: The LIGHT FIXTURE DETAILS on drawings 48 & 49 show a brick in each light base – are these existing bricks or will they have to be provided under this bid?

ANSWER: It is anticipated the existing lights have bricks in the structure which may be reused. If no brick is in the structure, a new brick shall be installed at no additional cost.

30. QUESTION: Specification GP-150, Section 34 notes “provide all flush mounted in-pavement light fixtures with snow plow rings”. The details on drawings 49 & 51 do not show a snow plow ring (for clarity) however they are not accurate for a snow plow ring installation. Please provide corrected details. Do all of the existing flush mounted lights have snow plow rings? Can you provide pictures of the existing installation?

ANSWER: It is anticipated all of the existing lights have snow-plow rings.

31. QUESTION: Drawing 51 EXISTING SHALLOW CAN IN-PAVEMENT LIGHT has notes that state “New L-864 cable installed in sawkerf with backer rod and sealer or existing ¾” IMC conduit between shallow cans” and note 5 states “Removal and reinstallation of backer rod and sealant shall be considered incidental to the installation of the new cable (NO SEPARATE PAY ITEM)”. Bid item 27 - NO. 10 AWG, 600V, L-824, TYPE C CABLE INSTALLED IN TRENCH, DUCT BANK OR CONDUIT -14,000 LF does not address “installed in sawkerf”. A separate bid item is needed: NO 10 AWG, 600V, L824 TYPE C INSTALLED IN EXISTING SAWKERF– REPLACE BACKER ROD AND SEALANT - XX LF.

ANSWER: Reference is directed to the Specifications and Sheet 51 sections of Addendum No. One (1).

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32. QUESTION: There are no specifications for the sealant to be used in the sawkerf or the P605 to be used around the in-pavement lights.

ANSWER: Sealant, as required, shall in be in conformance with and paid under a new Item P-605 bid item. Reference is directed to Addendum No. One (1).

33. QUESTION: Bid Item 39 – L-861T(L) ELEVATED MITL IN PAVEMENT FIXTURE ON NEW BASE WITH NEW TRANSFORMER. Shouldn't this be L-861T(L) ELEVATED MITL FIXTURE ON NEW BASE WITH NEW TRANSFORMER, IN EXISTING PAVEMENT?

ANSWER: As noted, the pay item includes the installation of a new elevated MITL, including a new base, in an existing pavement.

34. QUESTION: There are approximately 36 new taxiway lights that have remote transformers (we assume that they are mounted on existing shallow bases). Due to additional labor and material, these lights should be in a separate bid item. L-861T(L) ELEVATED MITL FIXTURE ON NEW BASE WITH NEW REMOTE TRANSFORMER.

ANSWER: No new bases are required. The secondary cables will be measured and paid separately from the light fixtures. Remote transformers and their associated installation will be considered incidental to the cost of the light fixtures.

35. QUESTION: Drawing 61, 62 & 63 ELECTRICAL VAULT -these plans and sections are noted as "NTS" – not to scale – how are we to determine the conduit, cable tray and wire lengths without knowing what the scale is?

ANSWER: Approximate dimension of regulator room within the electrical vault is 28 feet wide by 28 feet long. The Contractor shall field verify all dimensions.

36. QUESTION: Drawing 61 KEY NOTES do not address the removal of the existing CCRs for circuits 1,2,3,4,6,7 & 8. KEY NOTE D2 address the removal of the CCRs for circuits A, B, C, D, E, F & G.

ANSWER: The Contractor shall be responsible for removal of CCRs serving Circuits 1,2,3,4,6,7, and 8.

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37. **QUESTION:** Drawing 61 KEY NOTE M4 -SERVICE CONDUIT REROUTE...  
What size is the conduit and wire?
- ANSWER:** For bidding purposes, it shall be assumed that there is a 2-inch conduit with four (4) #3/0 AWG conductors. The Contractor shall field verify conduit and electrical service.
38. **QUESTION:** Specification L-109-1.1 notes painting of equipment and conduit – please clarify scope of work.
- ANSWER:** Conduit and electrical junction boxes shall be painted white to match vault walls.
39. **QUESTION:** Drawing 62 ELECTRICAL VAULT SAFETY BOARD – please provide a manufacture & catalog number (as an example) of what equipment is required.
- ANSWER:** Project is open bid and sole sourcing is not allowed, therefore specific manufactures or item numbers cannot be provided.
40. **QUESTION:** Drawing 62 KEY NOTE N3 notes the new cable tray for the power and controls for the CCRs. There are no specifications or description of the cable tray, except for the size 16”x4”.
- ANSWER:** Provide ladder type cable tray to match existing cable tray.
41. **QUESTION:** Drawing 62 & 63 – what elevation, above the finished floor, of the existing and new cable tray?
- ANSWER:** The existing cable tray is approximately 13 feet above the finished floor. The Contractor shall field verify all dimensions.
42. **QUESTION:** What is the ceiling height of the vault building?
- ANSWER:** The height of regulator room varies from approximately 12 feet on the south wall to approximately 28 feet on the north wall. The Contractor shall field verify all dimensions.

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43. QUESTION: Drawing 63 KEY NOTE N7 describes a S-1 cabinet. Drawing 66 shows details of a 5KV BREAKOUT PATCH PANEL ENCLOSURE. Are these the same enclosure?
- ANSWER: Yes. See plan revisions to Sheet 63 included in Addendum No. One (1).
44. QUESTION: Drawing 62 KEY NOTE N1 states “See CCR schedule and riser for additional information”. Where is the CCR schedule shown?
- ANSWER: See plan revisions on Sheet 62 included in Addendum No. One (1).
45. QUESTION: Drawing 63 KEY NOTE N16 states “1” electrical conduit for branch circuits... from cable tray to each CCR”. What does “branch circuit” refer to?
- ANSWER: Voltage driven circuit from Panelboard “MDP” or Panelboard “X” as defined by NFPA 70 NEC.
46. QUESTION: Drawing 64 NOTATION “UL 1449 4th edition surge protection device. Type to match circuit”. Please clarify and provide catalog number for what is required.
- ANSWER: Contractor shall field verify circuit phase (single/three phase) and voltage (120/240). SPD shall comply with UL1449 4<sup>th</sup> edition requirements.
47. QUESTION: Drawing 67 REVISED PANELBOARD MDP and REVISED PANELBOARD X, Note 1 states “all new breakers shall be compatible with existing panel AIC rating”. What is the AIC rating on panel MDP and panel X. There is a considerable price difference between a 10K AIC rated breaker and a 42K AIC rated breaker.
- ANSWER: The new breakers for Panel MDP shall be rated 42kAIC minimum. The new breakers for Panel X shall be rated 22kAIC min.



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48. QUESTION: Drawing 65 NOTE 3 states “All CCRs shall be capable and field modifiable for 3 step or 5 step operation. Provide replacement front cover switch for conversion for 3 step to 5 step or vice versa.” Is a replacement front cover switch required for each regulator?

ANSWER: No. Only seven (7) 3-step replacement cover switches are required.

49. QUESTION: There are numerous notes on the drawings with the notation (NO SEPARATE PAY ITEM), (NO PAY ITEM) or “considered incidental to ....”. These items should be address as part of the bid item that they are related to. For example, BID ITEM 2- REMOVE ELEVATED L-861T MITL FIXTURE AND TRANSFORMER. Drawing 13 shows remove fixture and install steel flush blank cover (TYP) (NO PAY ITEM). The New Bid Item should be – REMOVE ELEVATED L-861T MITL FIXTURE AND TRANSFORMER - INSTALL BLANK STEEL COVER.

ANSWER: This item will remain as previous detailed.

50. QUESTION: On Sheet 49, under LED RETROFIT DETAIL FOR RUNWAY & TAXIWAY SEMI-FLUSH EDGE LIGHT IN FLEXIBLE PAVEMENT, Note 4 indicates that all in-pavement lights are to be installed with snow plow rings. Are you able to confirm that snow plow rings are already in place on all in-pavement lights? If so, are the existing rings to remain in place or are they to be replaced? (If snow rings are to be replaced, the existing pavement may be damaged and will need to be repaired.) If snow rings are not in place or are in poor condition, are new rings to be installed? If so, are new rings considered incidental to the cost of the fixture and could you provide appropriate part numbers for each instance?

ANSWER: The existing in-pavement lights have snowplow rings. The existing rings are to remain in place. It is not anticipated that new snow-plow rings will be required.

51. QUESTION: On Sheet 40, for Light Nos. B193-195 (as well as other in-pavement lights), are the existing cans load-bearing or non-load bearing? If they are not load-bearing, are snow plow rings still applicable?

ANSWER: All in-pavement lights are load bearing and snow-plow rings are still applicable. The existing snow-plow rings may remain in-place.

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52. QUESTION: On Sheet 27, for entrance to Runway 6-24, the plans show ten (10) 852S(L) and the Bid Schedule shows ten (10) 852G(L). Which is correct?

ANSWER: Reference is directed to the plan revisions section of Addendum One (1).

53. QUESTION: Due to the logistics and prohibitively expensive cost associated with patching full depth pavement with asphalt, will cement back-fill flush with finished grade with a readily applied black coating be acceptable?

ANSWER: No.

54. QUESTION: Are marked-up, as-built plans sufficient or will you require survey data on new signs and windcones?

ANSWER: Survey data is not required.

55. QUESTION: Please elaborate on the frequency of concrete testing that will be required for sign pads, wind cone foundations, light bases and trenches? P-610-4.1 indicates the Engineer will sample concrete in accordance with ASTM C172... Will the Engineer handle all sampling and testing? Will the Contractor be responsible for any sampling and testing?

ANSWER: In accordance with Item P-610-4.1, Quality Assurance sampling and testing, the Engineer will perform the required acceptance testing.

56. QUESTION: Is a borrow pit available on-site for minor dress up around sign pads and windcones?

ANSWER: No.

57. QUESTION: Is an on-site disposal area available for concrete sign and wind cone foundations? Concrete can be crushed, and conduits and cans removed prior to disposal if this would facilitate on-site disposal.

ANSWER: As noted in the Plans and Specifications, all waste material must be disposed of off airport property.

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58. QUESTION: GP 30-12 indicates that:

*All work preliminary to the actual start of labor at the Airport must commence within ten (10) calendar days and completed within Sixty (60)calendar days of the effective date of the first Notice to Proceed.*

Paragraph 3 in the Contract indicates:

*All preliminary and administrative work shall be completed within One Hundred Twenty (120)consecutive calendar days after the effective date of the written Phase I Administrative Services Notice to Proceed.*

A milestone time of 120 days was also mention on the pre-bid call. Please clarify.

ANSWER: The Phase 1 Administrative Services shall be One-Hundred Twenty (120) days. Reference is directed to the Specification revisions and Revised Bid Forms of Addendum No. One (1).

59. QUESTION: Please provide photographs of the vault interior (same as shown in pre-bid call) as well as condition of paint, markings and pavement in and around Taxiway D.

ANSWER: Reference is directed to the Pre-Bid Meeting Presentation that were made available on the public bid-room following the conclusion of the meeting.

60. QUESTION: Please quantify the number of CONSTRUCTION AHEAD signs that are anticipated for the project similar to runway X's and taxiway barricades. Consider adding a Bid Schedule item similar to M-103 and M-107.

ANSWER: It is anticipated that four (4) construction ahead signs will be required as shown on the "CONSTRUCTION AHEAD SIGN LAYOUT" detail.

61. QUESTION: Given the small quantity of paint on the project, please confirm that a retro-reflectance test will be required as described in 620-3.8.

ANSWER: Yes, all testing shall be in accordance with the Specifications.

**ATTACHMENT NO. ONE (1)**

62. QUESTION: The specs call for photometric testing. I have looked and cannot find any other reference to the testing that is required. We are trying to determine how to quote this requirement and wondering about a few things. Details of the testing requirements and test reports (usually in the spec)? Who is to provide the testing vehicle? Type of lights to test? (usually in plan set) How many lights to test? (either in spec or plan set) Do we let you know which fixtures we can test or is it assumed that all of the new fixtures must be tested?

ANSWER: Photometric testing is required for all directional runway edge and in-pavement light fixtures after installation. Non-directional taxiway light fixtures and signs are not required to be tested after installation. Equipment, including vehicles, required for testing is incidental “methods and means” and shall be the responsibility of the contractor.

63. QUESTION: Please consider additional Bid Schedule Items for substantial work, e.g. Photometric Testing, Saw Kerf (2825’), that must otherwise be spread over existing Bid Schedule Items.

ANSWER: As noted in Addendum No. One (1), a pay item has been added for Joint Sealant to account for any replacement of cable in existing saw kerfs. No additional payment is proposed for photometric testing; the cost shall remain incidental to the associated bid items.

64. QUESTION: Please confirm that ETR means Existing to Remain, e.g. Sheet 62.

ANSWER: Yes.

65. QUESTION: Please clarify applicability of Trench 7 on Sheet 45 for Non-Aircraft Rated Pavement versus Full Depth Pavement Detail on Sheet 59. New elevated taxiway lights (at existing Taxiway D) will be installed in full depth pavement which will no longer need to be aircraft rated. (A previously submitted question asked if concrete in-fill with a readily applied black coating would be acceptable. This was in reference to the new elevated taxiway lights at existing Taxiway D.)

ANSWER: Trench 7 and the “Full Depth Pavement” detail are applicable to the new lights at existing Taxiway D. Reference is directed to Sheets 20 and 22.

**ATTACHMENT NO. ONE (1)**

66. QUESTION: On Sheets 52-53, the sign drawing indicates that leg lengths shall be adjusted to accommodate the slope of the sign base. This is a non-standard specification; this would potentially require the manufacturer to provide different length legs for each sign which would potentially challenge compliance with the frangibility requirements of the sign legs. Please explain your requirement and how it should be reconciled to the frangibility requirements.

ANSWER: The sign foundation shall be provided with a 0.5% - 1.0% slope across its length. It is anticipated that the slope will be varied as required to allow the sign top to be installed level. Any minor adjustments to leg length should not require separate manufacture. All frangibility requirements must be met.

67. QUESTION: On Page 51, Note 5 indicates the use of #6, 5kV cable. There is no Bid Schedule item for #6, 5kV cable. Should Note 5 indicate #8, 5kV cable?

ANSWER: Yes. Reference is directed to the Sheet 51 revisions including in Addendum No. One (1).

68. QUESTION: The validity period is specified on BF-3 as 90 days. In the pre-bid meeting, 120 days was discussed. In addition, section GP 30-02 indicates 120 days. Please resolve the discrepancy. Note: the contractor would prefer 90 days.

ANSWER: The validity period shall be One-Hundred Twenty (120) calendar days. Reference is directed to the Revised Bid Forms of Addendum No. One (1).

69. QUESTION: If existing base cans are mis-aligned and special products, e.g. wedge spacers, azimuth correction rings, etc., are required to correct such misalignment, will this work be considered incidental or will it be compensated as extra work? If such special products cannot be used to correct base can mis alignment, i.e. on existing in-pavement lights, base can replacement may be required to bring the light compliant with specifications. Is the contractor required to perform such work and how will the contractor be paid for such work?

ANSWER: Replacement of pieces or items to adjust the lights shall be incidental to the project. The L-868B, 12", CLASS IA LIGHT BASE per each bid item is to be used if it is determined that the entire can needs to be replaced during construction.

**ATTACHMENT NO. ONE (1)**

70. QUESTION: If snowplow rings must be replaced and existing bolts are broken or galled, will the repair work be considered incidental or will any extra work be covered under a change order?

ANSWER: The work shall be considered incidental.

71. QUESTION: The noted 2-piece wedge-type washers are typically only used in in-pavement applications as elevated lights have a gasket. Is the requirement that all bolts on all lights utilize 2-piece wedge-type washers or only bolts on in-pavement lights?

ANSWER: The 2-piece wedge-type washers are to be used on in-pavement lights as shown on the "ASSEMBLY DETAIL FOR LIGHT BASE IN FLEXIBLE PAVEMENT" on Sheet 49 of the plans.

**REVISED INVITATION TO BID**  
**BID NO. 20-001**

The Roanoke Regional Airport Commission will accept sealed bids for furnishing all labor, materials, and equipment and performing all work for:

REHABILITATE AIRFIELD SIGNAGE & LIGHTING  
AT  
ROANOKE BLACKSBURG REGIONAL AIRPORT  
AIP GRANT NO. 3-51-0045-062-2019 (DESIGN)  
AIP PROJECT NO. 3-51-0045-PENDING (CONSTRUCTION)

The project involves the rehabilitation of the Airport's taxiway and runway lighting, signage, and wind cones with new LED fixtures, along with new transformers and cable. The rehabilitation of the existing airfield electrical vault will include the installation of new Constant Current Regulators (CCRs), S-1 cutout panel, and conduit infrastructure.

Bids shall be received until 11:00 A.M. on Friday, June 12, 2020, in the Commission Office at the Roanoke-Blacksburg Regional Airport, 5202 Aviation Drive, Roanoke, Virginia 24012. Bids will be publicly opened and read aloud at that time in Conference Room A on the Second Floor of the Airport Terminal Building.

The Bid Documents may be examined at Valley Construction News, 426 W. Campbell Avenue, Roanoke, Virginia. Bid Documents may be purchased at <https://bidders.deltaairport.com>. Plans and Specifications are available for purchase in both hardcopy and digital (PDF) formats. No refunds will be granted. Contact Delta Airport Consultants, Inc. at (804) 275-8301.

Bidders are invited to submit bids for this work on the bid forms provided in the package; other bid forms will not be accepted. The successful bidder shall be required to have and maintain a Class A Virginia Contractor's License and not less than \$5,000,000 in general liability and motor vehicle insurance. Contractor, its employees and any subcontractors employees will be required to submit to a fingerprint-based criminal records check and be approved for access to and within the airport secured and sterile areas and must be and remain approved for access to and within the airport secured and sterile areas

Each bid must be accompanied by a bid security in a form acceptable to the Commission in an amount equal to at least five percent (5%) of the amount of the bid by the Contractor, payable to the Roanoke Regional Airport Commission, as a guaranty that if the bid is accepted, the bidder will execute the Contract and file required Performance and Payment Bonds within the time provided in the Instructions to Bidders.

If the bid by the lowest responsible bidder exceeds funds allocated for the project, the Commission reserves the right to negotiate with the apparent low bidder pursuant to the terms set out in the Instructions to Bidders. The Commission additionally reserves the right to reject any

and all bids, and to accept any part of or combination of bids, to waive any informalities or irregularities in any bid, and to award the Contract to other than the lowest bidder, should it be deemed to be in the best interest of the Commission. If a contract is awarded to other than the low bidder, it will be awarded to the lowest responsible and responsive bidder.

This project is funded with federal, state, and local monies and the Contractor shall be subject to all laws and regulations applicable to the recipients of such funds. All bidders must be licensed as Virginia Class A contractors. Bidders shall include a copy of their licensing certificate in the bid package. Minority business enterprises will be afforded full opportunity to submit bids in response to this Invitation and will not be discriminated against on the ground of race, color, or national origin in consideration for an award.

The Roanoke Regional Airport Commission in accordance with the provisions of Title VI of the Civil Rights Act of 1964 (78 Stat. 252, 42 U.S.C. §2000d-2000d-4) and the Regulations, hereby notifies all bidders that it will affirmatively ensure that any contract entered into pursuant to this advertisement, disadvantaged business enterprises will be afforded full and fair opportunity to submit bids in response to this invitation and will not be discriminated against on the grounds of race, color, or national origin in consideration for an award. All bidders shall endeavor to afford Disadvantaged Business Enterprises (DBE's) a reasonable opportunity to participate in this project. The Commission's goal for DBE participation is 2.4%. All bidders wishing to remain in competition for the contract shall submit documentation of their DBE goal accomplishments or good faith efforts in accordance with the bid documents.

A Virtual Pre-Bid meeting has been scheduled for 2:00 P.M., May 26, 2020 using the GoToWebinar platform. Instructions for attending the Virtual Meeting are included in the Pre-Bid Meeting section of the specifications. Attendance by bidders is encouraged to discuss the project design and the proposed construction, however it is not mandatory.



## Item P-605 Joint Sealants for Pavements

### DESCRIPTION

**605-1.1** This item shall consist of providing and installing a resilient and adhesive joint sealing material capable of effectively sealing joints in pavement; joints between different types of pavements; and cracks in existing pavement.

### MATERIALS

**605-2.1 Joint sealants.** Joint sealant materials shall meet the requirements of ASTM D5893 or ASTM D6690.

Each lot or batch of sealant shall be delivered to the jobsite in the manufacturer's original sealed container. Each container shall be marked with the manufacturer's name, batch or lot number, the safe heating temperature, and shall be accompanied by the manufacturer's certification stating that the sealant meets the requirements of this specification.

**605-2.2 Backer rod.** The material furnished shall be a compressible, non-shrinking, non-staining, non-absorbing material that is non-reactive with the joint sealant in accordance with ASTM D5249. The backer-rod material shall be  $25\% \pm 5\%$  larger in diameter than the nominal width of the joint.

**605-2.3 Bond breaking tapes.** Provide a bond breaking tape or separating material that is a flexible, non-shrinkable, non-absorbing, non-staining, and non-reacting adhesive-backed tape. The material shall have a melting point at least  $5^{\circ}\text{F}$  ( $3^{\circ}\text{C}$ ) greater than the pouring temperature of the sealant being used when tested in accordance with ASTM D789. The bond breaker tape shall be approximately 1/8 inch (3 mm) wider than the nominal width of the joint and shall not bond to the joint sealant.

### CONSTRUCTION METHODS

**605-3.1 Time of application.** Joints shall be sealed as soon after completion of the curing period as feasible and before the pavement is opened to traffic, including construction equipment. ~~The pavement temperature shall be  $50^{\circ}\text{F}$  ( $10^{\circ}\text{C}$ ) and rising at the time of application of the poured joint sealing material.~~ Do not apply sealant if moisture is observed in the joint. Weather limitations shall be in accordance with manufacturers recommendations.

**605-3.2 Equipment.** Machines, tools, and equipment used in the performance of the work required by this section shall be approved before the work is started and maintained in satisfactory condition at all times. Submit a list of proposed equipment to be used in performance of construction work including descriptive data, 14 days prior to use on the project.

**605-3.3 Preparation of joints.** Pavement joints for application of material in this specification must be dry, clean of all scale, dirt, dust, curing compound, and other foreign matter. The Contractor shall demonstrate, in the presence of the Engineer RPR, that the method cleans the joint and does not damage the joint.

**a. Sawing.** All joints shall be sawed in accordance with specifications and plan details. Immediately after sawing the joint, the resulting slurry shall be completely removed from joint and adjacent area by flushing with a jet of water, and by use of other tools as necessary.

**b. Sealing.** Immediately before sealing, the joints shall be thoroughly cleaned of all remaining laitance, curing compound, filler, protrusions of hardened concrete, old sealant and other foreign material from the sides and upper edges of the joint space to be sealed. Cleaning shall be accomplished by waterblaster as specified in paragraph 605-3.2. The newly exposed concrete joint faces and the pavement surface extending a minimum of 1/2 inch (12 mm) from the joint edge shall be sandblasted clean. Sandblasting shall be accomplished in a minimum of two passes. One pass per joint face with the nozzle held at an angle directly toward the joint face and not more than 3 inches (75 mm) from it. After final cleaning and immediately prior to sealing, blow out the joints with compressed air and leave them completely free of debris and water. The joint faces shall be surface dry when the seal is applied.

**c. Backer Rod.** When the joint opening is of a greater depth than indicated for the sealant depth, plug or seal off the lower portion of the joint opening using a backer rod in accordance with paragraph 605-2.2 to prevent the entrance of the sealant below the specified depth. Take care to ensure that the backer rod is placed at the specified depth and is not stretched or twisted during installation.

**d. Bond-breaking tape.** Where inserts or filler materials contain bitumen, or the depth of the joint opening does not allow for the use of a backup material, insert a bond-separating tape breaker in accordance with paragraph 605-2.3 to prevent incompatibility with the filler materials and three-sided adhesion of the sealant. Securely bond the tape to the bottom of the joint opening so it will not float up into the new sealant.

**605-3.4 Installation of sealants.** Joints shall be inspected for proper width, depth, alignment, and preparation, and shall be approved by the Engineer RPR before sealing is allowed. Sealants shall be installed in accordance with the following requirements:

Immediately preceding, but not more than 50 feet (15 m) ahead of the joint sealing operations, perform a final cleaning with compressed air. Fill the joints from the bottom up to 1/8 inch  $\pm$  1/16 inch below the top of pavement surface; or bottom of groove for grooved pavement. Remove and discard excess or spilled sealant from the pavement by approved methods. Install the sealant in such a manner as to prevent the formation of voids and entrapped air. In no case shall gravity methods or pouring pots be used to install the sealant material. Traffic shall not be permitted over newly sealed pavement until authorized by the Engineer RPR. When a primer is recommended by the manufacturer, apply it evenly to the joint faces in accordance with the manufacturer's instructions. Check the joints frequently to ensure that the newly installed sealant is cured to a tack-free condition within the time specified.

**605-3.5 Inspection.** The Contractor shall inspect the joint sealant for proper rate of cure and set, bonding to the joint walls, cohesive separation within the sealant, reversion to liquid, entrapped air and voids. Sealants exhibiting any of these deficiencies at any time prior to the final acceptance of the project shall be removed from the joint, wasted, and replaced as specified at no additional cost to the airport.

**605-3.6 Clean-up.** Upon completion of the project, remove all unused materials from the site and leave the pavement in a clean condition.

## METHOD OF MEASUREMENT

**605-4.1** Joint sealing material shall be measured by the linear foot of sealant in place, completed, and accepted.

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**BASIS OF PAYMENT**

**605-5.1** Payment for joint sealing material shall be made at the contract unit price per linear foot. The price shall be full compensation for furnishing all materials, for all preparation, delivering, and placing of these materials, and for all labor, equipment, tools, and incidentals necessary to complete the item.

Payment will be made under:

Item P-605-5.1            Joint Sealing Filler, per linear foot

**REFERENCES**

The publications listed below form a part of this specification to the extent referenced. The publications are referred to within the text by the basic designation only.

## ASTM International (ASTM)

ASTM D789            Standard Test Method for Determination of Relative Viscosity of Polyamide (PA)

ASTM D5249        Standard Specification for Backer Material for Use with Cold- and Hot-Applied Joint Sealants in Portland-Cement Concrete and Asphalt Joints

ASTM D5893        Standard Specification for Cold Applied, Single Component, Chemically Curing Silicone Joint Sealant for Portland Cement Concrete Pavements

ASTM D6690        Standard Specification for Joint and Crack Sealants, Hot Applied, for Concrete and Asphalt

## Advisory Circulars (AC)

AC 150/5340-30    Design and Installation Details for Airport Visual Aids

**END ITEM P-605**

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**SUMMARY OF QUANTITIES**

ITEM NO.	SPEC NO.	DESCRIPTION	UNIT	AS-BID	AS-BUILT
1	C-105	MOBILIZATION	LS	1	
2	P-150	REMOVE ELEVATED L-861T MITL FIXTURE AND TRANSFORMER	EA	669	
3	P-150	REMOVE IN-PAVEMENT L-861T MITL FIXTURE AND TRANSFORMER	EA	3	
4	P-150	REMOVE ELEVATED L-862 HIRL FIXTURE AND TRANSFORMER	EA	105	
5	P-150	REMOVE ELEVATED L-862E HIRL FIXTURE AND TRANSFORMER	EA	40	
6	P-150	REMOVE IN-PAVEMENT L-850C HIRL FIXTURE AND TRANSFORMER	EA	29	
7	P-150	REMOVE ELEVATED L-804 RGL FIXTURE AND TRANSFORMER	EA	2	
8	P-150	REMOVE IN-PAVEMENT L-852G RGL FIXTURE AND TRANSFORMER	EA	10	
9	P-150	REMOVE AIRFIELD GUIDANCE SIGN AND FOUNDATION	EA	118	
10	P-150	REMOVE L-806 SECONDARY WINDCONE AND FOUNDATION	EA	3	
11	P-150	REMOVE L-807 PRIMARY WINDCONE AND FOUNDATION	EA	1	
12	P-150	REMOVE CABLE FROM EXISTING CONDUIT OR DUCT	LF	220,000	
13	M-103	CLOSED RUNWAY MARKER (OWNER-FURNISHED)	EA	2	
14	M-103	CLOSED RUNWAY MARKER (CONTRACTOR-FURNISHED)	EA	2	
15	M-103	CLOSED TAXIWAY MARKER (CONTRACTOR-FURNISHED)	EA	4	
16	M-107	AVIATION BARRICADES (CONTRACTOR-FURNISHED)	LF	3,000	
17	P-605	JOINT SEALING FILLER	LF	1,500	
18	P-619	PAINT REMOVAL (95-100% LEVEL)	SF	11,000	
19	P-620	MARKING (PERMANENT YELLOW)	SF	1,600	
20	P-620	MARKING (PERMANENT BLACK)	SF	1,500	
21	P-620	REFLECTIVE MEDIA	LS	1	
22	D-752	6" UNDERDRAIN ENDWALL, VDOT STD. EW-12	EA	2	
23	T-901	SEEDING	SY	10,000	
24	T-908	MULCHING	SY	10,000	
25	L-107	L-806(L), STYLE I-B, SIZE 1 SECONDARY WIND CONE AND FOUNDATION	EA	3	
26	L-107	L-807(L), STYLE I-B, SIZE 2 PRIMARY WIND CONE AND FOUNDATION	EA	1	
27	L-108	NO. 8 AWG, 5KV, L-824, TYPE "C" CABLE, INSTALLED IN TRENCH, DUCT BANK OR CONDUIT	LF	200,000	
28	L-108	NO. 10 AWG, 600V, L-824, TYPE "C" CABLE, INSTALLED IN TRENCH, DUCT BANK OR CONDUIT	LF	14,000	
29	L-108	NO. 10 AWG, 600V, XHHN, INSTALLED IN TRENCH, DUCT BANK OR CONDUIT	LF	1,000	
30	L-108	NO. 6 AWG, SOLID, BARE COPPER COUNTERPOISE, INSTALLED IN TRENCH, DUCT BANK OR CONDUIT	LF	6,000	
31	L-109	VAULT EQUIPMENT MODIFICATIONS	LS	1	
32	L-110	NON-ENCASED ELECTRICAL CONDUIT, 1 WAY - 2" PVC CONDUIT	LF	5,000	
33	L-110	NON-ENCASED ELECTRICAL CONDUIT, BORED UNDER PAVEMENT, 2 WAY - 2" HDPE CONDUIT	LF	100	
34	L-110	NON-ENCASED ELECTRICAL CONDUIT TRENCHED THROUGH EXISTING PAVEMENT, 1 WAY - 2" PVC CONDUIT	LF	900	
35	L-110	NON-ENCASED ELECTRICAL DRAINAGE CONDUIT, 1 WAY - 2" PVC CONDUIT	LF	500	
36	L-115	ELECTRICAL JUNCTION STRUCTURE - L-867 PULLCAN	EA	30	
37	L-119	REPLACE AIRPORT OBSTRUCTION LIGHT FIXTURE (LED), IN PLACE	EA	19	
38	L-119	INSTALL AIRPORT OBSTRUCTION LIGHT (LED)	EA	1	
39	L-119	INSTALL SOLAR AIRPORT OBSTRUCTION LIGHT (LED)	EA	1	
40	L-125	L-861(L) ELEVATED MITL (LED) FIXTURE ON EXISTING BASE WITH NEW TRANSFORMER	EA	650	
41	L-125	L-861(L) ELEVATED MITL (LED) IN-PAVEMENT FIXTURE ON NEW BASE WITH NEW TRANSFORMER	EA	8	
42	L-125	L-852(L) IN-PAVEMENT MITL (LED) FIXTURE ON EXISTING BASE WITH NEW TRANSFORMER	EA	3	
43	L-125	L-862(L) ELEVATED HIRL (LED) FIXTURE ON EXISTING BASE WITH NEW TRANSFORMER	EA	106	
44	L-125	L-862E(L) ELEVATED THRESHOLD HIRL (LED) FIXTURE ON EXISTING BASE WITH NEW TRANSFORMER	EA	40	
45	L-125	L-850C(L) IN-PAVEMENT HIRL (LED) FIXTURE ON EXISTING BASE WITH NEW TRANSFORMER	EA	28	
46	L-125	L-804(L) ELEVATED RGL (LED) FIXTURE ON EXISTING BASE WITH NEW TRANSFORMER	EA	2	
47	L-125	L-852G(L) IN-PAVEMENT RGL (LED) FIXTURE ON EXISTING BASE WITH NEW TRANSFORMER	EA	10	
48	L-125	L-868B, 12", CLASS IA LIGHT BASE	EA	5	
49	L-125	L-858(L) AIRFIELD GUIDANCE SIGN (1-2 CHAR.) ON NEW FOUNDATION	EA	8	
50	L-125	L-858(L) AIRFIELD GUIDANCE SIGN (3-4 CHAR.) ON NEW FOUNDATION	EA	53	
51	L-125	L-858(L) AIRFIELD GUIDANCE SIGN (5-6 CHAR.) ON NEW FOUNDATION	EA	20	
52	L-125	L-858(L) AIRFIELD GUIDANCE SIGN (7-8 CHAR.) ON NEW FOUNDATION	EA	3	
53	L-125	L-858(L) AIRFIELD GUIDANCE SIGN (RW 34 DEPARTURES) ON NEW FOUNDATION	EA	1	
54	L-125	L-858(L) AIRFIELD GUIDANCE SIGN (1-2 CHAR.) ON NEW FOUNDATION IN EXISTING PAVEMENT	EA	2	
55	L-125	L-858(L) AIRFIELD GUIDANCE SIGN (3-4 CHAR.) ON NEW FOUNDATION IN EXISTING PAVEMENT	EA	14	
56	L-125	L-858(L) AIRFIELD GUIDANCE SIGN (5-6 CHAR.) ON NEW FOUNDATION IN EXISTING PAVEMENT	EA	4	
57	L-125	L-858(L) AIRFIELD GUIDANCE SIGN (7-8 CHAR.) ON NEW FOUNDATION IN EXISTING PAVEMENT	EA	1	
58	L-125	L-858(L) RUNWAY DISTANCE REMAINING SIGN ON NEW FOUNDATION	EA	8	
59	L-125	TEMPORARY L-858 AIRFIELD GUIDANCE SIGN PANEL	EA	124	
60	GP-150-32	REHABILITATE SEGMENTED CIRCLE	LS	1	


























DRAWING: 19013qty.dwg LAYOUT: ADD1 2-1

# ELECTRICAL SYMBOL LEGEND

## SYMBOL STATUS






















<u>EXISTING</u>	<u>REMOVE</u>	<u>NEW</u>
		

## SYMBOL DESCRIPTION

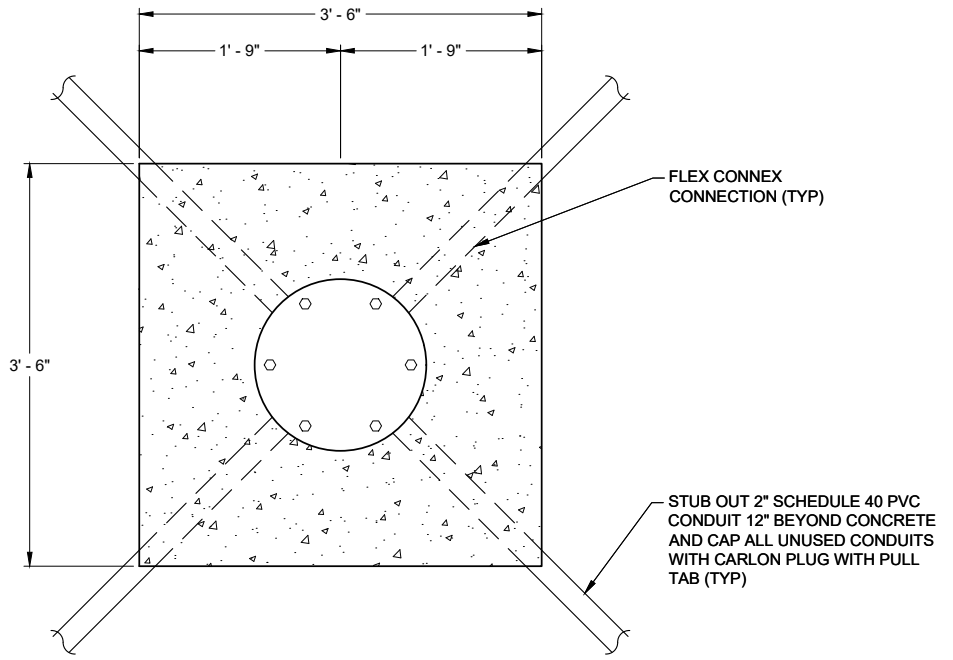
	L-852G(L) IN-PAVEMENT RGL (LED)
W  W	L-850C(L) IN-PAVEMENT HIRL
	L-852T(L) IN-PAVEMENT OMNI-DIRECTIONAL MITL
	L-858(L) AIRFIELD GUIDANCE SIGN ON NEW FOUNDATION
	L-858(L) AIRFIELD GUIDANCE SIGN ON NEW FOUNDATION WITH TEMPORARY PANELS (SEE SIGN SCHEDULE)
	L-861T(L) BASE MOUNTED MITL ON EXISTING BASE CAN
 N	L-861T(L) BASE MOUNTED MITL ON NEW BASE CAN
 R	L-861T(L) BASE MOUNTED MITL (RED LENSES) ON EXISTING BASE CAN
	L-862(L) BASE MOUNTED HIRL ON EXISTING BASE CAN
R  G	L-862E(L) BASE MOUNTED THRESHOLD LIGHT ON EXISTING BASE CAN
	L-867 ELECTRICAL PULLCAN
	L-867 ELECTRICAL PULLCAN PLAZA (# INDICATES NUMBER OF CANS)
	ELECTRICAL HANDHOLE
	ELECTRICAL JUNCTION BOX
	ELECTRICAL MANHOLE
	L-804(L) ELEVATED RUNWAY GUARD LIGHT
	EXISTING ELECTRICAL PULLCAN FOR FUTURE L-804(L) ELEVATED RUNWAY GUARD LIGHT
	L-801(L) OBSTRUCTION LIGHT
	L-806 / L-807 WIND CONE
	NEW CABLE IN EXISTING CONDUIT
	NEW CABLE & CONDUIT
	NEW CABLE IN EXISTING DUCT BANK - BURIED UNDER PAVEMENT (CONCRETE ENCASED)
	DUCT BANK - BORED UNDER PAVEMENT
	DRAINAGE CONDUIT
	FIBER OPTIC CABLE

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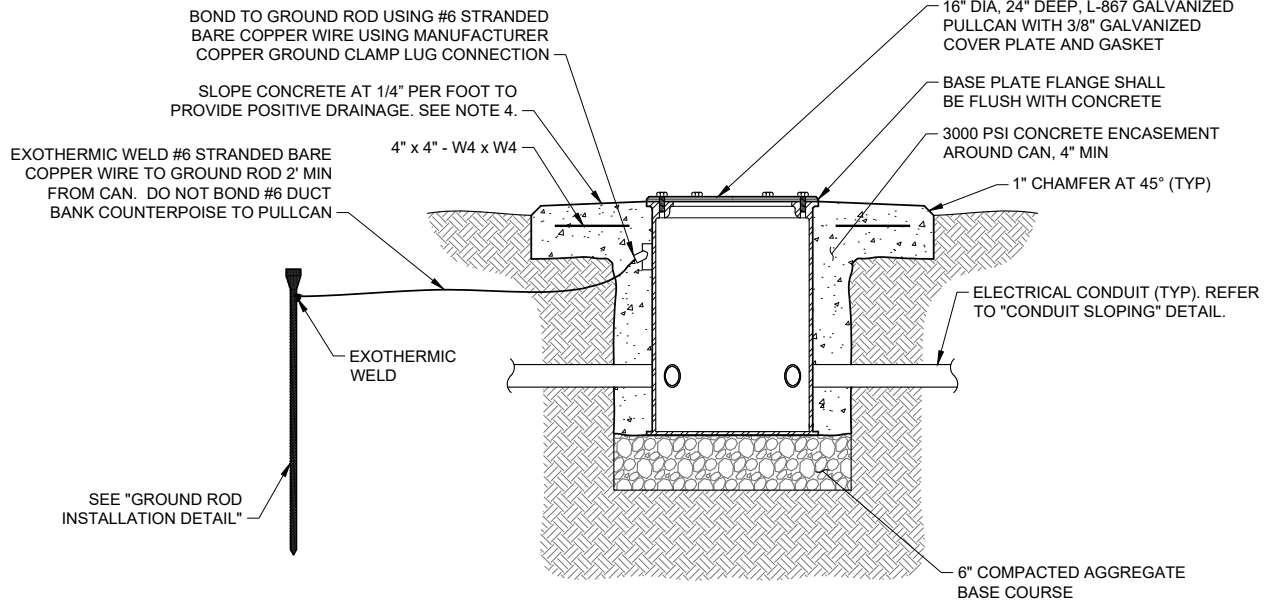
# LEGEND

	WORK AREA/PHASE LABEL
<hr/>	
	WORK AREA/ PHASING LIMITS
	L-852G(L) IN-PAVEMENT RGL (LED)
W  W	L-850C(L) IN-PAVEMENT HIRL
	L-852T(L) IN-PAVEMENT OMNI-DIRECTIONAL MITL
	L-858(L) AIRFIELD GUIDANCE SIGN ON NEW FOUNDATION
	L-858(L) AIRFIELD GUIDANCE SIGN ON NEW FOUNDATION WITH TEMPORARY PANELS (SEE SIGN SCHEDULE)
	L-861T(L) BASE MOUNTED MITL ON EXISTING BASE CAN
	L-861T(L) BASE MOUNTED MITL ON NEW BASE CAN
	L-861T(L) BASE MOUNTED MITL (RED LENSES) ON EXISTING BASE CAN
	L-862(L) BASE MOUNTED HIRL ON EXISTING BASE CAN
R 	L-862E(L) BASE MOUNTED THRESHOLD LIGHT ON EXISTING BASE CAN
	L-867 ELECTRICAL PULLCAN
	L-867 ELECTRICAL PULLCAN PLAZA (# INDICATES NUMBER OF CANS)
	ELECTRICAL HANDHOLE
	ELECTRICAL JUNCTION BOX
	ELECTRICAL MANHOLE
	L-804(L) ELEVATED RUNWAY GUARD LIGHT
	EXISTING ELECTRICAL PULLCAN FOR FUTURE L-804(L) ELEVATED RUNWAY GUARD LIGHT
	L-801(L) OBSTRUCTION LIGHT
	COUNTERPOISE GROUND ROD
	L-806 / L-807 WIND CONE
<hr style="width: 100px; display: inline-block; vertical-align: middle;"/> RSA	EXISTING RUNWAY SAFETY AREA

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**PLAN VIEW**



**VERTICAL SECTION VIEW**

**PULLCAN DETAIL**

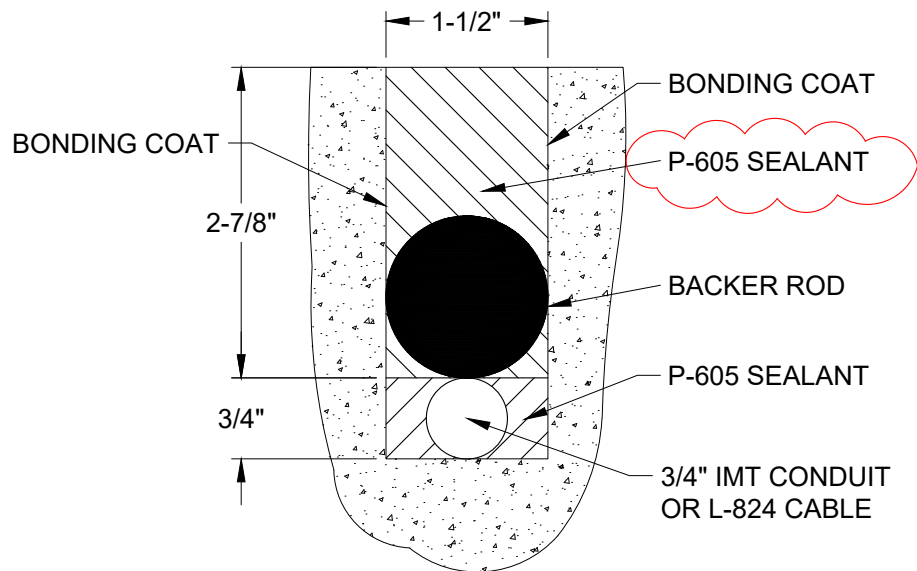
NTS

**NOTES:**

1. PULLCANS, CONCRETE, CONDUIT, W4 x W4, AGGREGATE, GROUND ROD, COUNTERPOISE, EXCAVATION, AND BACKFILL ARE INCIDENTAL TO THE COST OF THE PULLCAN PLAZA.
2. ALL DIRECT BURIED BARE WIRE CONNECTIONS SHALL BE EXOTHERMIC WELDS ONLY. LUG CONNECTIONS SHALL BE USED FOR CONCRETE ENCASED APPLICATIONS.
3. CONCRETE SURFACE SHALL RECEIVE A BROOM FINISH.
4. CONTRACTOR SHALL LABEL EACH CAN LID FOR CIRCUIT IDENTIFICATION. SEE "PULLCAN LID LABEL DETAIL". SPARE PULLCANS SHALL NOT BE LABELED.

DRAWING: 19013e1d.dwg LAYOUT: ADD 47-1



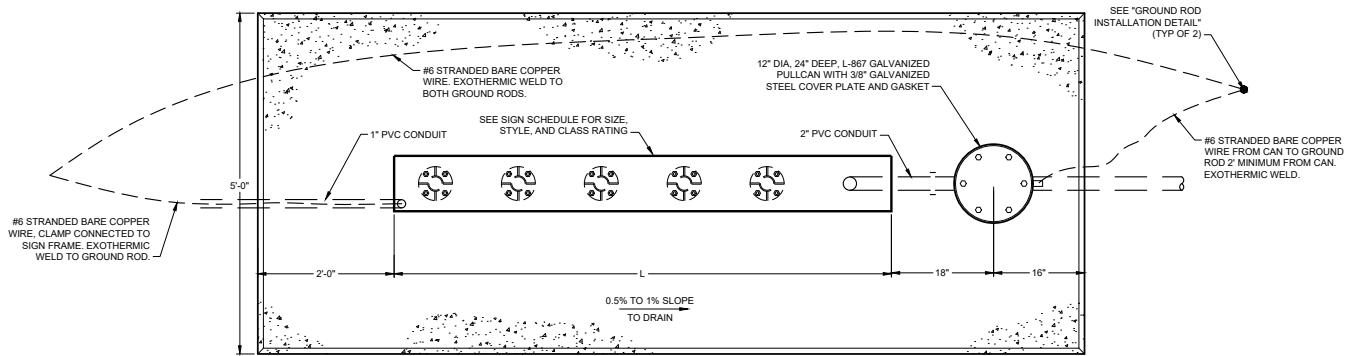


## SAW KERF WIREWAY

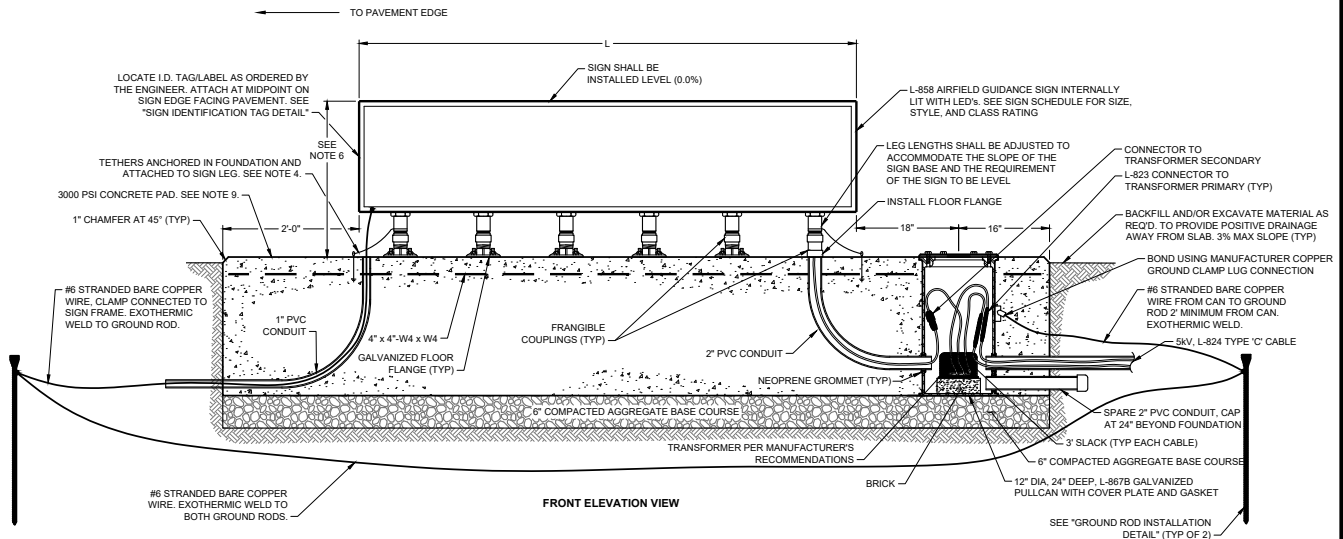
NTS

### NOTES:

1. CONTRACTOR SHALL VERIFY EXISTING FIELD CONDITIONS PRIOR TO CONSTRUCTION.
2. REMOVAL AND REPLACEMENT OF BACKER ROD AND SEALANT IN EXISTING SAW KERFS SHALL BE PAID PER LINEAR FOOT UNDER P-605 "JOINT SEALING FILLER".



PLAN VIEW



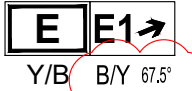



FRONT ELEVATION VIEW

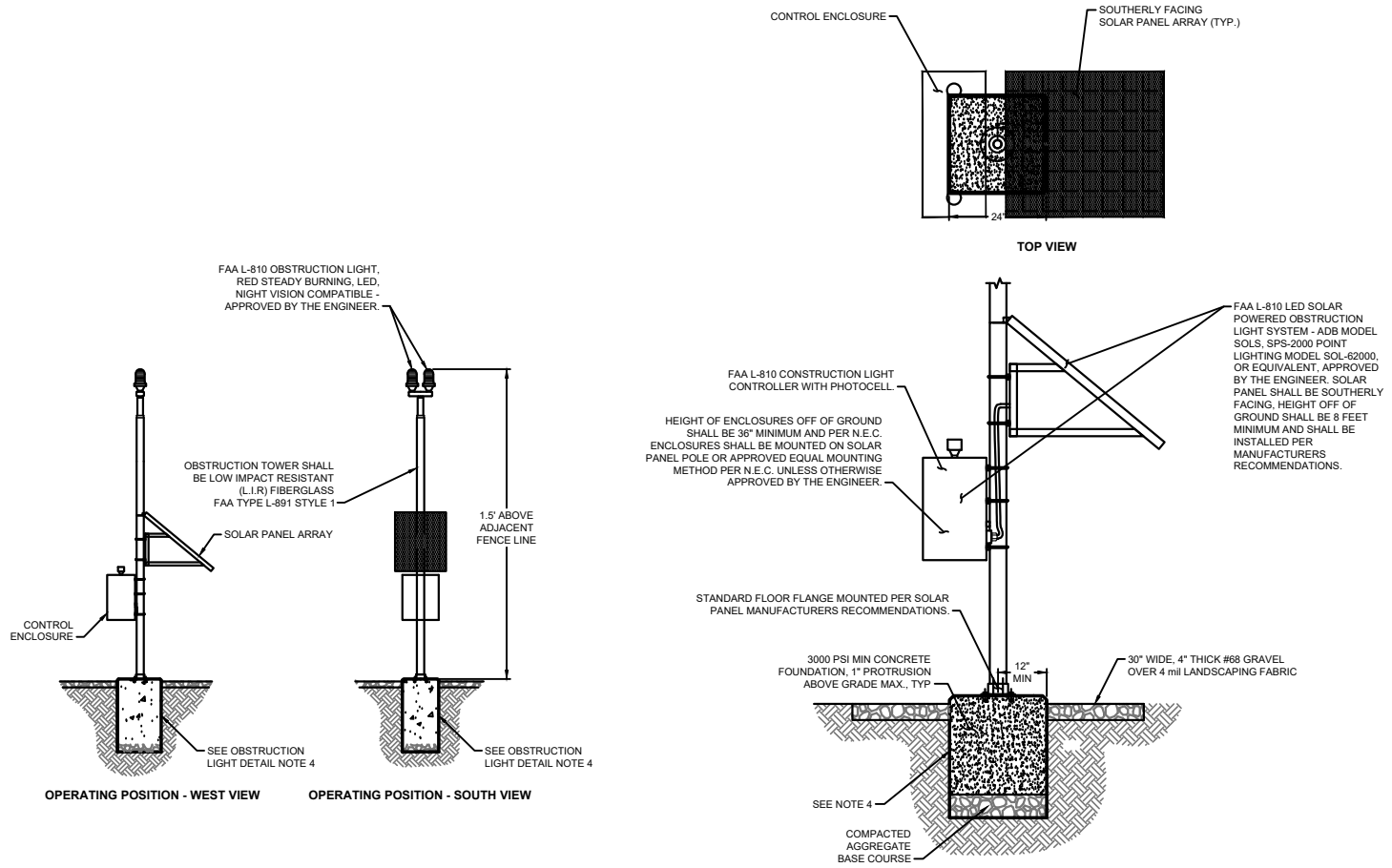
**LED AIRFIELD GUIDANCE SIGN DETAIL  
(OPTIONAL PRECAST PAD)**

NTS

NOTES:

1. PROVIDE A L-823 DISCONNECT PLUG & RECEPTACLE IN THE FRANGIBLE COUPLING OF THE POWER LEG AS REQUIRED.
2. ALL CONDUIT UNDER THE CONCRETE PAD IS INCIDENTAL TO THE COST OF THE AIRFIELD GUIDANCE SIGN.
3. ORIENT THE INTERNAL POWER LUGS OF THE AIRFIELD GUIDANCE SIGN SO THAT THEY WILL BE CLOSEST TO THE LEG THROUGH WHICH THE POWER ENTERS THE SIGN HOUSING.
4. SINGLE MODULE SIGN REQUIRES ONE TETHER. MULTIPLE MODULE SIGN IN CONTINUOUS FRAME MUST USE TETHER AT BOTH ENDS.
5. ALL DIRECT BURIED BARE WIRE CONNECTIONS SHALL BE EXOTHERMIC WELDS ONLY. LUG CONNECTIONS SHALL BE USED FOR CONCRETE ENCASED APPLICATIONS.
6. 30" MAX. (SIZE 1), 36" MAX. (SIZE 2), OR 42" MAX. (SIZE 3). THESE DIMENSIONS APPLY TO THE SIDE CLOSEST TO THE PAVEMENT. OPPOSITE SIDE WILL BE GREATER TO ACCOMMODATE SLOPE OF SLAB.
7. CONTRACTOR TO COORDINATE DEPTH OF PAD WITH MANUFACTURER'S RECOMMENDED ANCHOR BOLTS.
8. PROVIDE EXTERNAL POWER DISCONNECT (ON/OFF) SWITCH TO ISOLATE THE SIGN FROM THE CIRCUIT FOR MAINTENANCE.
9. CONCRETE SURFACE SHALL RECEIVE A BROOM FINISH.
10. THE USE OF PRECAST PADS IS AT THE CONTRACTOR'S OPTION; NO ADDITIONAL PAYMENT WILL BE MADE FOR THE USE OF PRECAST PADS.

SIGN SCHEDULE					
SIGN #	TEMPORARY SIGN PANEL INFORMATION				
	PANEL "A"		PANEL "B"		PHASE #
	INSCRIPTION	TYPE	INSCRIPTION	TYPE	
132		L-858L / Y		L-858L	7B
133		L-858L		L-858Y / L	7B
134					



**SOLAR OBSTRUCTION LIGHT DETAIL**

NTS

NOTES:

1. CONCRETE SLAB SHALL BE 3,000 PSI STRENGTH.
2. LAYOUT OF FOUNDATION AND ANCHOR BOLTS SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S WRITTEN INSTALLATION INSTRUCTIONS.
3. ALL WORK SHOWN IS INCIDENTAL TO THE UNIT PRICE PER EACH FOR "INSTALL SOLAR AIRPORT OBSTRUCTION LIGHT (LED)".
4. OBSTRUCTION LIGHT TOWER FOUNDATION DETAIL SHOWN HERE IS FOR GENERAL INFORMATION PURPOSES ONLY. TOWER FOUNDATION SHALL BE DESIGNED TO WITHSTAND A LOADING PER POLE MANUFACTURERS RECOMMENDATIONS. THE INFORMATION SHOWN IS REPRESENTATIVE OF TYPICAL OBSTRUCTION LIGHT TOWER FOUNDATION.
5. OBSTRUCTION TOWER SHALL BE LOW IMPACT RESISTANT (L.I.R) FIBERGLASS AND IN ACCORDANCE WITH FAA TYPE L-891 STYLE 1.
6. ALL EQUIPMENT SHALL BE LISTED IN THE LATEST EDITION OF APPENDIX I OF FAA ADVISORY CIRCULAR 150/5345-53D, AIRPORT LIGHTING EQUIPMENT CERTIFICATION PROGRAM. SOLAR POWER SYSTEM SHALL MEET THE REQUIREMENTS OF FAA ENGINEERING BRIEFS 67 AND 76. MANUFACTURERS OF LIGHTS AND OF SOLAR POWER SYSTEM SHALL PROVIDE CERTIFICATION EACH IS COMPATIBLE AND THAT SYSTEM IS ABLE TO POWER BOTH LIGHTS FROM "DUSK TO DAWN" ON SHORTEST DAY OF THE YEAR.

			15 KW	6.6 A	3	X-15,17	(NEW TW B) TW E (NEW TW J)	GULF T/W ECHO	---
	6G	# G	7.5 KW	6.6 A	3	X-15,17	(NEW TW B) TW E (NEW TW J)	GULF T/W ECHO	---
NEW WORK	6H	# 1A	15 KW	6.6 A	5 (NOTE	MDP-12	TW SPARE	CCR # 1A	TW SPARE
	6H	# 1B	15 KW	6.6 A	5 (NOTE	MDP-14	TW SPARE	CCR # 1B	TW SPARE
	6H	# 2A	15 KW	6.6 A	5 (NOTE	MDP-16	RW SPARE	CCR # 2A	RW SPARE
	6H	# 2B	4 KW	6.6 A	5 (NOTE	X-20,22	GUARD LTS	CCR # 2B	RW GUA LIGHTS
	6H	# 3A	7.5 KW	6.6 A	5 (NOTE	X-27,29	TW T	CCR # 3A	TW TANGC
	6H	# 3B	4 KW	6.6 A	5 (NOTE	X-16,18	APRON	CCR # 3B	TW APRON
	6F	# 4A	7.5 KW	6.6 A	5 (NOTE 3)	X-19,21	TW J	CCR # 4A	TW JULIET
	6F	# 4B	15 KW	6.6 A	5 (NOTE 3)	X-23,25	TW B	CCR # 4B	TW BRAVO
	6D	# 5A	10 KW	6.6 A	5 (NOTE 3)	MDP-1	RW 6-24 NE (24)	CCR # 5A	RW 6-2 NORTH E 24 END
	6D	# 5B	10 KW	6.6 A	5 (NOTE 3)	MDP-3	RW 6-24 SW (6)	CCR # 5B	RW 6-2 SOUTH W 6 END
	6B	# 6A	15 KW	6.6 A	5 (NOTE	MDP-4	RW 16-34	CCR # 6A	RW 16-
	6B	# 6B	7.5 KW	6.6 A	5 (NOTE	MDP-11	TW A - N+S	CCR # 6B	TW ALP NORTH-SC
	FUTURE	# 7A	---	---	---	---	FUTURE	CCR # 7A	---
	6B	# 7B	15 KW	6.6 A	5 (NOTE	MDP-5	TW A - MID	CCR # 7B	TW ALP MIDFIELD
	FUTURE	# 8A	---	---	---	---	FUTURE	CCR # 8A	---
FUTURE	# 8B	---	---	---	---	FUTURE	CCR # 8B	---	

**NOTES:**

1. ALL NEW LABELS SHALL BE INDOOR/OUTDOOR UV STABLE WITH SELF-ADHESIVE BACKING
2. NEW CCR POSITION SUFFIX "A" INDICATES ABOVE AND SUFFIX "B" INDICATES BELOW
3. ALL CCR'S SHALL BE CAPABLE AND FIELD MODIFIABLE FOR 3 STEP OR 5 STEP OPERATION. PROVIDE REPLACEMENT
4. LABEL 1 SHALL BE WHITE TEXT ON BLACK BACKGROUND
5. FIELD APPLY COLOR CODED ELECTRICAL TAPE TO EACH CABLE ENTERING AND EXITING PULLING STRUCTURE WHERE REQUIRED, PLACE 2ND "WHITE" ID COLOR BAND WITHIN 2 INCHES OF FIRST COLOR.
6. CIRCUITS #B, #C, & #D MUST BE TEMPORARILY SERVED DURING PHASES 6G AND 6H UNTIL INSTALLATION OF
7. DO NOT CONNECT NEW CCR UNTIL ENTIRE CIRCUIT HAS BEEN CONVERTED TO LED FIXTURES TO ENSURE CO

DRAWING: 19013vlt.dwg LAYOUT: ADD1 65-1

REVISED BID FORMS

\_\_\_\_\_  
(Name of Bidding Contractor)  
For

REHABILITATE AIRFIELD SIGNAGE & LIGHTING

AT

ROANOKE-BLACKSBURG REGIONAL AIRPORT  
ROANOKE, VIRGINIA

SUBMITTED TO THE

ROANOKE REGIONAL AIRPORT COMMISSION  
ROANOKE, VIRGINIA

RRAC BID NO. 20-001

AIP PROJECT NO. 3-51-0045-062-2019 (DESIGN)  
AIP PROJECT NO. 3-51-0045-PENDING (CONSTRUCTION)

In compliance with the Invitation for Bids, the undersigned hereby proposes to furnish the materials and labor and to perform the work for the completion of the Rehabilitate Airfield Lights and Signs Project in strict accordance with the Invitation for Bids, Instructions to Bidders, Specifications (including the General Provisions, Special Provisions, Supplemental Provisions, and Construction Details), Plans, Addenda and all other contract documents for the consideration of the prices quoted below in this bid form , and agrees, upon receipt of written notice of award, that it will execute a contract in accordance with the bid as accepted and give the required contract bonds with good and sufficient surety, within fifteen (15) calendar days after receipt of notice of formal award of contract and presentation of the prescribed forms.

It is agreed that the undersigned has informed itself fully in regard to all conditions pertaining to the place where the work is to be done; that it has examined the plans and specifications for the work and contractual documents thereto, including the special provisions, prior to the opening of bids, and that it has satisfied itself relative to the work to be performed.

It is agreed that the description under each item, being stated, implies, although it does not mention, all incidentals and that the price(s) stated are intended to cover all such work, materials, labor, equipment and incidentals as constitute the bidder's obligations as described in the specifications, and any details not specifically mentioned, but evidently included in the contract, shall be compensated for in the item which most logically includes it.

## REVISED BID FORMS

It is understood that this bid is submitted for the purpose of obtaining the work included in subject project at the Roanoke-Blacksburg Regional Airport.

Said work is described in the project Bid and Contract documents which also include the place, date, and time of opening bids.

It is understood that separate contracts on individual schedules of work may be awarded, when included in the bid documents.

It is understood that wages not less than the minimum rates or wages, as predetermined for this project by the U.S. Secretary of Labor, were used in the preparation of this bid.

It is understood that all workmanship and materials under all items of work are warranted for one (1) year from the date of final acceptance, or such longer time as the manufacturer shall specify or these bid/contract documents may require.

It is understood that the Owner reserves the right to accept or reject any or all bids and waive informalities.

It is understood that the items and quantities of work to be done are approximate only and are intended principally to serve as a guide in evaluation of bids.

The undersigned agrees that if awarded the contract, it will commence and complete the work in accordance with the provision, requirements and deadlines of Section 30-12 of the General Conditions.

Contract Time: One Hundred Twenty (120) consecutive calendar days from Phase 1 Administrative Services Notice to Proceed and Three Hundred Thirty (330), consecutive calendar days from Phase 2 Performance of Work Notice to Proceed.

It is understood and agreed that for each calendar day that the work remains incomplete after the contract time and/or the milestone times (including all extensions and adjustments as provided in the Contract Documents), the amount per day as specified in Section 4, Contract Sum and Liquidated Damages of the form Contract included with these specifications, shall be liquidated damages and may be retained, deducted and/or offset from any amounts due or to become due to the Contractor or its Surety. Such liquidated damages shall not be a penalty, but shall be considered as an agreed liquidation of a reasonable portion of damages that will be incurred by Owner as a result of the Contractor failing to complete the Work in the time provided in the Contract Documents. It is understood and agreed that: (a) the actual damages that may result from failure to complete the Work within the required time are uncertain and difficult to determine with exactness and that the fixed amount is not out of proportion to the probable loss; (b) Owner retains the right to make such retentions, deductions and/or offsets for liquidated damages at any time and that Owner's imposition and the retention, deduction and/or offset of any liquidated damages hereunder shall not be subject to any prior notice or claim requirements; and, (c) by submitting this Bid, Bidder acknowledges and agrees that Bidder waives any defenses as to the validity of any liquidated damages provisions in this Contract

## REVISED BID FORMS

based on such liquidated damages being void as penalties or not being reasonably related to actual damages. It is further agreed, however, that application of liquidated damages hereunder shall not be Owner's exclusive remedy and shall not bar any other claim, cause of action, or remedy that Owner may have against Bidder under applicable law in the performance of this Contract.

It is understood that this project is funded by federal, state, and local monies and the Contractor shall be subject to all laws and regulations applicable to recipients of such funds.

Enclosed is security as required, consisting of \_\_\_\_\_(cash, escrow, certified check, or bid bond) payable to the Roanoke Regional Airport Commission, in the amount of \$\_\_\_\_\_.

This amount equals five percent of the total amount bid submitted by the Contractor.

The Contractor shall be a licensed Class A Contractor registered with the Commonwealth of Virginia, shall list its registration number at the end of this bid in the designated location **and shall enclose a copy of its licensing certificate.**

The Contractor shall complete all items and fill in all blanks on these bid form pages.

This bid will remain valid and binding on Bidder for a period of One Hundred Twenty (120) days from date of bid opening.



**REVISED BID FORMS**

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## REVISED BID FORMS

AIP Project No. 3-51-0045-062-2019 (Design)  
 AIP Project No. 3-51-0045-Pending (Construction)  
 RRAC Bid No. 20-001  
 Delta Project No. 19013

**BASE BID**

Date: \_\_\_\_\_

**Bid Proposal Summary For All Work Depicted In The Plans And Specifications**

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
1 C-105	1 LS	MOBILIZATION     PER LUMP SUM		
2 P-150	669 EA	REMOVE ELEVATED L-861T MITL FIXTURE AND TRANSFORMER     PER EACH		
3 P-150	3 EA	REMOVE IN-PAVEMENT L-861T MITL FIXTURE AND TRANSFORMER     PER EACH		
4 P-150	105 EA	REMOVE ELEVATED L-862 HIRL FIXTURE AND TRANSFORMER     PER EACH		
5 P-150	40 EA	REMOVE ELEVATED L-862E HIRL FIXTURE AND TRANSFORMER     PER EACH		
6 P-150	29 EA	REMOVE IN-PAVEMENT L-850C HIRL FIXTURE AND TRANSFORMER     PER EACH		
7 P-150	2 EA	REMOVE ELEVATED L-804 RGL FIXTURE AND TRANSFORMER     PER EACH		
8 P-150	10 EA	REMOVE IN-PAVEMENT L-852G RGL FIXTURE AND TRANSFORMER     PER EACH		
9 P-150	118 EA	REMOVE AIRFIELD GUIDANCE SIGN AND FOUNDATION     PER EACH		

## REVISED BID FORMS

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
10 P-150	3 EA	REMOVE L-806 SECONDARY WINDCONE AND FOUNDATION  _____ _____ PER EACH		
11 P-150	1 EA	REMOVE L-807 PRIMARY WINDCONE AND FOUNDATION  _____ _____ PER EACH		
12 P-150	220,000 LF	REMOVE CABLE FROM EXISTING CONDUIT OR DUCT  _____ _____ PER LINEAR FOOT		
13 M-103	2 EA	CLOSED RUNWAY MARKER (OWNER-FURNISHED)  _____ _____ PER EACH		
14 M-103	2 EA	CLOSED RUNWAY MARKER (CONTRACTOR-FURNISHED)  _____ _____ PER EACH		
15 M-103	4 EA	CLOSED TAXIWAY MARKER (CONTRACTOR-FURNISHED)  _____ _____ PER EACH		
16 M-107	3,000 LF	AVIATION BARRICADES (CONTRACTOR-FURNISHED)  _____ _____ PER LINEAR FOOT		
17 P-605	1,500 LF	JOINT SEALING FILLER  _____ _____ PER LINEAR FOOT		
18 P-619	11,000 SF	PAINT REMOVAL (95-100% LEVEL)  _____ _____ PER SQUARE FOOT		

## REVISED BID FORMS

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
19 P-620	1,600 SF	MARKING (PERMANENT YELLOW) _____ _____ _____ PER SQUARE FOOT		
20 P-620	1,500 SF	MARKING (PERMANENT BLACK) _____ _____ _____ PER SQUARE FOOT		
21 P-620	1 LS	REFLECTIVE MEDIA _____ _____ _____ PER LUMP SUM		
22 D-752	2 EA	6" UNDERDRAIN ENDWALL, VDOT STD. EW-12 _____ _____ _____ PER EACH		
23 T-901	10,000 SY	SEEDING _____ _____ _____ PER SQUARE YARD		
24 T-908	10,000 SY	MULCHING _____ _____ _____ PER SQUARE YARD		
25 L-107	3 EA	L-806(L), STYLE I-B, SIZE 1 SECONDARY WIND CONE AND FOUNDATION _____ _____ _____ PER EACH		
26 L-107	1 EA	L-807(L), STYLE I-B, SIZE 2 PRIMARY WIND CONE AND FOUNDATION _____ _____ _____ PER EACH		
27 L-108	200,000 LF	NO. 8 AWG, 5KV, L-824, TYPE "C" CABLE, INSTALLED IN TRENCH, DUCT BANK OR CONDUIT _____ _____ _____ PER LINEAR FOOT		

**REVISED BID FORMS**

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
28 L-108	14,000 LF	NO. 10 AWG, 600V, L-824, TYPE "C" CABLE, INSTALLED IN TRENCH, DUCT BANK OR CONDUIT  _____ _____ PER LINEAR FOOT		
29 L-108	1,000 LF	NO. 10 AWG, 600V, XHHN, INSTALLED IN TRENCH, DUCT BANK OR CONDUIT  _____ _____ PER LINEAR FOOT		
30 L-108	6,000 LF	NO. 6 AWG, SOLID, BARE COPPER COUNTERPOISE, INSTALLED IN TRENCH, DUCT BANK OR CONDUIT  _____ _____ PER LINEAR FOOT		
31 L-109	1 LS	VAULT EQUIPMENT MODIFICATIONS  _____ _____ PER LUMP SUM		
32 L-110	5,000 LF	NON-ENCASED ELECTRICAL CONDUIT, 1 WAY - 2" PVC CONDUIT  _____ _____ PER LINEAR FOOT		
33 L-110	100 LF	NON-ENCASED ELECTRICAL CONDUIT, BORED UNDER PAVEMENT, 2 WAY - 2" HDPE CONDUIT  _____ _____ PER LINEAR FOOT		
34 L-110	900 LF	NON-ENCASED ELECTRICAL CONDUIT TRENCHED THROUGH EXISTING PAVEMENT, 1 WAY - 2" PVC CONDUIT  _____ _____ PER LINEAR FOOT		
35 L-110	500 LF	NON-ENCASED ELECTRICAL DRAINAGE CONDUIT, 1 WAY - 2" PVC CONDUIT  _____ _____ PER LINEAR FOOT		
36 L-115	30 EA	ELECTRICAL JUNCTION STRUCTURE - L-867 PULLCAN  _____ _____ PER EACH		

**REVISED BID FORMS**

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
37 L-119	19 EA	REPLACE AIRPORT OBSTRUCTION LIGHT FIXTURE (LED), IN PLACE _____ _____ PER EACH		
38 L-119	1 EA	INSTALL AIRPORT OBSTRUCTION LIGHT (LED) _____ _____ PER EACH		
39 L-119	1 EA	INSTALL SOLAR AIRPORT OBSTRUCTION LIGHT (LED) _____ _____ PER EACH		
40 L-125	650 EA	L-861T(L) ELEVATED MITL (LED) FIXTURE ON EXISTING BASE WITH NEW TRANSFORMER _____ _____ PER EACH		
41 L-125	8 EA	L-861T(L) ELEVATED MITL (LED) IN-PAVEMENT FIXTURE ON NEW BASE WITH NEW TRANSFORMER _____ _____ PER EACH		
42 L-125	3 EA	L-852T(L) IN-PAVEMENT MITL (LED) FIXTURE ON EXISTING BASE WITH NEW TRANSFORMER _____ _____ PER EACH		
43 L-125	106 EA	L-862(L) ELEVATED HIRL (LED) FIXTURE ON EXISTING BASE WITH NEW TRANSFORMER _____ _____ PER EACH		
44 L-125	40 EA	L-862E(L) ELEVATED THRESHOLD HIRL (LED) FIXTURE ON EXISTING BASE WITH NEW TRANSFORMER _____ _____ PER EACH		
45 L-125	28 EA	L-850C(L) IN-PAVEMENT HIRL (LED) FIXTURE ON EXISTING BASE WITH NEW TRANSFORMER _____ _____ PER EACH		

**REVISED BID FORMS**

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
46 L-125	2 EA	L-804(L) ELEVATED RGL (LED) FIXTURE ON EXISTING BASE WITH NEW TRANSFORMER _____ _____ PER EACH		
47 L-125	10 EA	L-852G(L) IN-PAVEMENT RGL (LED) FIXTURE ON EXISTING BASE WITH NEW TRANSFORMER _____ _____ PER EACH		
48 L-125	5 EA	L-868B, 12", CLASS IA LIGHT BASE _____ _____ PER EACH		
49 L-125	8 EA	L-858(L) AIRFIELD GUIDANCE SIGN (1-2 CHAR.) ON NEW FOUNDATION _____ _____ PER EACH		
50 L-125	53 EA	L-858(L) AIRFIELD GUIDANCE SIGN (3-4 CHAR.) ON NEW FOUNDATION _____ _____ PER EACH		
51 L-125	20 EA	L-858(L) AIRFIELD GUIDANCE SIGN (5-6 CHAR.) ON NEW FOUNDATION _____ _____ PER EACH		
52 L-125	3 EA	L-858(L) AIRFIELD GUIDANCE SIGN (7-8 CHAR.) ON NEW FOUNDATION _____ _____ PER EACH		
53 L-125	1 EA	L-858(L) AIRFIELD GUIDANCE SIGN (RW 34 DEPARTURES) ON NEW FOUNDATION _____ _____ PER EACH		
54 L-125	2 EA	L-858(L) AIRFIELD GUIDANCE SIGN (1-2 CHAR.) ON NEW FOUNDATION IN EXISTING PAVEMENT _____ _____ PER EACH		

## REVISED BID FORMS

ITEM NO.	APPROX. QUANTITY	ITEM WITH UNIT PRICE WRITTEN IN WORDS	UNIT PRICES IN FIGURES	EXTENDED TOTAL
			DOLLARS CENTS	DOLLARS CENTS
55 L-125	14 EA	L-858(L) AIRFIELD GUIDANCE SIGN (3-4 CHAR.) ON NEW FOUNDATION IN EXISTING PAVEMENT  _____ _____ PER EACH		
56 L-125	4 EA	L-858(L) AIRFIELD GUIDANCE SIGN (5-6 CHAR.) ON NEW FOUNDATION IN EXISTING PAVEMENT  _____ _____ PER EACH		
57 L-125	1 EA	L-858(L) AIRFIELD GUIDANCE SIGN (7-8 CHAR.) ON NEW FOUNDATION IN EXISTING PAVEMENT  _____ _____ PER EACH		
58 L-125	8 EA	L-858(L) RUNWAY DISTANCE REMAINING SIGN ON NEW FOUNDATION  _____ _____ PER EACH		
59 L-125	124 EA	TEMPORARY L-858 AIRFIELD GUIDANCE SIGN PANEL  _____ _____ PER EACH		
60 GP-150-32	1 LS	REHABILITATE SEGMENTED CIRCLE  _____ _____ PER LUMP SUM		

Contract Time: 330 Calendar Days

Liquidated Damages: \$2,000.00/Calendar Day

Total Base Bid Amount \$ \_\_\_\_\_

SP-1 Non-AIP Insurance Allowance \$ \_\_\_\_\_

THE BIDDER SHALL COMPLETE ALL ITEMS AND FILL IN ALL BLANKS IN THIS BID FORM.



**REVISED BID FORMS**

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REVISED BID FORMS

CONTRACTOR'S CERTIFICATION OF NONSEGREGATED FACILITIES

The federally assisted construction contractor certifies that it does not maintain or provide, for its employees, any segregated facilities at any of its establishments and that it does not permit employees to perform services at any location, under its control, where segregated facilities are maintained. The federally assisted construction contractor certifies that it will not maintain or provide, for its employees, segregated facilities at any of its establishments and that it will not permit its employees to perform services at any location, under its control, where segregated facilities are maintained. The federally assisted construction contractor agrees that a breach of this certification is a violation of the Equal Opportunity Clause in this contract.

As used in this certification, the term "segregated facilities" means any waiting room, work areas, restrooms and washrooms, restaurants and other eating areas, timeclocks, locker rooms and other storage or dressing areas, parking lots, drinking fountains, recreation or entertainment areas, transportation, and housing facilities provided for employees which are segregated by explicit directives or are in fact segregated on the basis of race, color, religion, or national origin because of habit, local custom, or any other reason. The federally assisted construction contractor agrees that (except where it has obtained identical certifications from proposed subcontractors for specific time periods) it will obtain identical certifications from proposed subcontractors prior to the award of subcontracts exceeding \$10,000 which are not exempt from the provisions of the equal opportunity clause and that it will retain such certifications in its files.

The information above is true and complete to the best of my knowledge.

\_\_\_\_\_  
Name and Title of Authorized Official (please print or type)

\_\_\_\_\_  
Signature

\_\_\_\_\_  
Date

NOTE: The penalty for making false statements in offers is prescribed in 18 U.S.C. 1001

REVISED BID FORMS

CONTRACTOR'S CERTIFICATION OF ELIGIBILITY

By entering into this contract, the Contractor certifies that neither it (not he or she) nor any person or firm who has an interest in the Contractor's firm is a person or firm ineligible to be awarded Government contracts by virtue of Section 3(a) of the Davis-Bacon Act or 29 CFR 5.12 (a)(1).

That, no part of this contract shall be subcontracted to any person or firm ineligible for award of a Government contract by virtue of the same citation;

The bidder/offeror certifies, by submission of this bid or acceptance of this contract, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency. It further agrees by submitting this bid that it will include this clause without modification in all lower tier transactions, solicitations, bids, contracts, and subcontracts. Where the bidder/offer/contractor or any lower tier participant is unable to certify to this statement, it shall attach an explanation to this solicitation/bid.

That, the information above is true and complete to the best of my knowledge.

---

Name and Title of Authorized Official (please print or type)

---

Signature

---

Date

NOTE: The penalty for making false statements in offers is prescribed in 18 U.S.C. 1001.

**REVISED BID FORMS**  
**CERTIFICATION FOR CONTRACTS, GRANTS, LOANS**  
**AND COOPERATIVE AGREEMENTS**

The undersigned certifies, to the best of his or her knowledge and belief, that:

(1) No Federal appropriated funds have been paid or will be paid; by or on behalf of the undersigned, to any person for influencing or attempting to influence an officer or employee of any agency, a member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with the awarding of any Federal contract, the making of any Federal grant, the making of any Federal Loan, the entering into of any cooperative agreement, and the extension, continuation, renewal, amendment, or modification of any Federal contract, grant, loan, or cooperative agreement.

(2) If any funds other than Federal appropriated funds have been paid or will be paid to any person for influencing or attempting to influence an officer or employee of any agency, a Member of Congress, an officer or employee of Congress, or an employee of a Member of Congress in connection with this Federal contract grant, loan, or cooperative agreement, the undersigned shall complete and submit Standard Form-LLL, "Disclosure of Lobby Activities," in accordance with its instructions.

(3) The undersigned shall require that the language of this certification be included in the award documents for all sub awards at all tiers (including subcontracts, subgrants, and contracts under grants, loans, and cooperative agreements) and that all sub recipients shall certify and disclose accordingly.

This certification is a material representation of fact upon which reliance was placed when this transaction was made or entered into. Submission of this certification is a prerequisite for making or entering into this transaction imposed by section 1352, title 31, U.S. Code. Any person who fails to file the required certification shall be subject to a civil penalty of not less than \$10,000 for each such failure.

\_\_\_\_\_  
Name and Title of Authorized Official (please print or type)

\_\_\_\_\_  
Signature

\_\_\_\_\_  
Date

**REVISED BID FORMS**

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## REVISED BID FORMS

### CONTRACTOR'S CERTIFICATION OF DBE PARTICIPATION

The apparent low bidder shall make good faith efforts, as defined in Appendix "A" of 49 CFR Part 26, Regulations of the Office of the Secretary of Transportation, to subcontract portions of the dollar value of the prime contract to small business concerns owned and controlled by socially and economically disadvantaged individuals (DBE). In the event that the bidder for this solicitation qualifies as a DBE, the contract goal shall be deemed to have been met. Individuals who are rebuttably presumed to be socially and economically disadvantaged include women, Blacks, Hispanics, Native Americans, Asian-Pacific Americans, and Asian-Indian Americans. **Within five (5) business days of being determined as the apparent low responsive and responsible bidder, such bidder shall be required to submit information concerning the DBE's that will participate in this contract in accordance with current applicable federal regulations. The information will include: (1) the name and address of each DBE; (2) a description of the work to be performed by each named firm; (3) the dollar value of the subcontract; (4) a copy of the firm's DBE Certificate or letter of certification award; (5) written and signed documentation of commitment by the bidder regarding the use of specific DBE subcontractor(s); and (6) written and signed confirmation from the DBE that it is participating in the contract as provided in the prime Contractor=s commitment. If the apparent low responsive and responsible bidder fails to achieve the contract goal stated herein, it shall also provide acceptable documentation within the same five (5) business day period demonstrating that it made meaningful good faith efforts in attempting to do so. An apparent low responsive and responsible bid that fails to meet these requirements may be considered to be non-responsive, in violation of its bid commitment and the Commission reserves the right to make claim under and/or forfeit such bidder's bid bond, pursue any other remedies available at law or in equity, and proceed to award to the next lowest responsive and responsible bidder.**

This requirement is imposed by federal regulation and is material to award of contract.

Qualifications and requirements for DBE selection and good faith efforts are generally described in the Instructions to Bidders.

A substitution of DBE Subcontractors may be permitted only upon written request and approval by the Roanoke Regional Airport Commission.

REVISED BID FORMS

Bidder assures and covenants that it will subcontract not less than \_\_\_\_\_% of the total contract dollars to Disadvantage Business Enterprise Firms. If less than the goal in Section 8.3 of the INSTRUCTION TO BIDDERS, evidence clearly demonstrating meaningful good faith efforts shall be submitted by the apparent low bidder, who is otherwise responsive and responsible, within five (5) business days after being notified that it is the apparent low bidder. The determination of whether the bidder has made a meaningful good faith effort shall be made by the Commission=s DBE Liaison Officer.

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Name and Title of Authorized Official (please print or type)

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Signature \_\_\_\_\_ Date \_\_\_\_\_

NOTE: The penalty for making false statements in offers is prescribed in 18 U.S.C. 1001.

REVISED BID FORMS

BIDDER CERTIFICATION  
OF LICENSURE AND LICENSURE OF SUBCONTRACTORS

Project: Rehabilitate Airfield Lights and Signs

The undersigned Bidder hereby covenants and agrees to comply with Title 54.1, Chapter 11, Code of Virginia (1950), as amended, with respect to licensure of Bidder and all subcontractors who may be employed to perform the Work for the Roanoke Regional Airport Commission.

Bidder further represents and covenants: (i) that Bidder has verified that all subcontractors, currently identified in the Bid to perform a portion of the Work, hold all required Commonwealth of Virginia and local licenses, including, without limitation, Contractor and business licenses; and, (ii) that if it is the Successful Bidder, Bidder shall verify that any additional subcontractors employed to perform the Work, subsequent to the date of this certification, shall hold all required Commonwealth of Virginia and local licenses, including, without limitation, Contractor and business licenses.

Bidder acknowledges and agrees that if it is awarded a contract for the Work, this Certification shall constitute a material part of Bidder's contract with the Commission and violation of the terms of this Certification shall constitute a breach of such Contract.

All persons executing this Certification on behalf of Bidder hereby warrant and represent that they have been duly authorized by proper action of Bidder to execute this Certification, and that upon such execution, this Certification shall be binding upon and enforceable against Bidder.

IN WITNESS WHEREOF, the Bidder has affixed its hand and seal.

\_\_\_\_\_  
(Insert Bidder's Name)

CONTRACTORS LICENSE NO: \_\_\_\_\_

By: \_\_\_\_\_

Title: \_\_\_\_\_

Date: \_\_\_\_\_



## REVISED BID FORMS C-3

Each contractor shall furnish with its bid the following completed and signed statement pertaining the Bidder's competency and responsibility in accordance with the General Conditions.

FIRM: \_\_\_\_\_  
ADDRESS: \_\_\_\_\_  
\_\_\_\_\_  
PHONE: \_\_\_\_\_ FAX: \_\_\_\_\_  
EMAIL: \_\_\_\_\_

Contact in your firm for inquiries: \_\_\_\_\_  
Years of business under present name: \_\_\_\_\_  
Date of Incorporation: \_\_\_\_\_  
Place of Incorporation: \_\_\_\_\_  
Contracting Specialties: \_\_\_\_\_

Is firm prequalified with the Virginia Department of Transportation, VDOT and on the current bidder's list?

Yes \_\_\_\_\_ No \_\_\_\_\_

If "Yes", submit evidence of such. If "No", submit a copy of the firm's annual report or financial statement for the last fiscal year (certified by a public accountant). Reference is directed to General Conditions Section 20-02.

Years performing work specialties: \_\_\_\_\_

Maximum Bonding Limits of firm: \_\_\_\_\_

List equipment available for project: \_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**Name of proposed on-site Superintendent and relevant project experience during last five (5) years: \_\_\_\_\_**

**(Complete next page for relevant project experience of proposed on-site project superintendent)**

# REVISED BID FORMS C-3

Relevant Project Experience for Proposed Superintendent

Type of Project and Date

Responsibilities

Contact Name/Phone No.

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**Bidder acknowledges and agrees that the Commission retains the absolute right to reject the above designated individual as the project superintendent for this work if it determines that the persons job experience and/or references are not adequate or good, and to require bidder to provide one or more alternative proposed superintendents, along with their relevant job experience and references, until the parties are in agreement as to the superintendent for the job.**

# REVISED BID FORMS C-3

Has Firm:

Failed to complete a contract? \_\_\_\_\_

Been involved in a bankruptcy or reorganization? \_\_\_\_\_

Pending judgment claims or suits against Firm? \_\_\_\_\_

(If answer is "yes" to any of the preceding,  
submit details on separate sheet).

List four (4) most recent contracts or subcontracts completed in the last five (5) years that included work similar to that required in this project.

<u>Type of Project</u>	<u>Contract With</u>	<u>Contract Amount</u>	<u>Date Completed</u>
_____	_____	_____	_____
_____	_____	_____	_____
_____	_____	_____	_____
_____	_____	_____	_____
_____	_____	_____	_____
_____	_____	_____	_____
_____	_____	_____	_____

\_\_\_\_\_  
Name and Title of Authorized Official (please print or type)

\_\_\_\_\_  
Signature

\_\_\_\_\_  
Date

# REVISED BID FORMS C-3

## CERTIFICATION OF NON-COLLUSION

My signature certifies that the accompanying bid is not the result of or affected by, any act of collusion with another person or company engaged in the same line of business or commerce, or any act of fraud punishable under Title 18.2, Chapter 12, Article 1.1 of the **Code of Virginia**, 1950, as amended. Furthermore, I understand that fraudulent and collusive bidding is a crime under the Virginia Governmental Frauds Act, the Virginia Government Bid Rigging Act, the Virginia Antitrust Act, and Federal Law and can result in fines, prison sentences, and civil damage awards.

I agree to abide by all conditions of this bid and certify that I am authorized to sign this bid for the bidder.

NAME OF BIDDER \_\_\_\_\_

ADDRESS \_\_\_\_\_

SIGNATURE \_\_\_\_\_

NAME (TYPE OR PRINT) \_\_\_\_\_

OFFICIAL TITLE \_\_\_\_\_

DATE \_\_\_\_\_

TELEPHONE NO. (\_\_\_\_) \_\_\_\_\_  
Area Code

EMAIL ADDRESS \_\_\_\_\_

"REGISTERED VIRGINIA CONTRACTOR"

NO. \_\_\_\_\_

**ATTACH COPY OF VIRGINIA CONTRACTOR'S LICENSE**

REVISED BID FORMS C-3

Revised 04/05/12

COMMONWEALTH OF VIRGINIA

WORKERS' COMPENSATION

Certificate of Coverage

Section 2.2-4332, Code of Virginia, requires construction contractors and Subcontractors to obtain and maintain workers' compensation insurance for the duration of the Work on behalf of the Commonwealth of Virginia, its departments, institutions or agencies, or local governmental entities.

Satisfactory evidence of coverage on this form must be provided to the Commission prior to commencement of work.

The undersigned organization stipulates that it:

A. Has workers' compensation insurance and is in compliance with the Workers' Compensation statues of the Commonwealth of Virginia \_\_\_ Yes \_\_\_ No

Insurance Company\_\_\_\_\_

Policy expiration date \_\_\_\_\_, or

B. Is self insured for workers' compensation \_\_\_\_\_ Yes.

Title of Construction Contract: Rehabilitate Airfield Signage & Lighting

Bid Number: 20-001

Signed by: \_\_\_\_\_

Title: \_\_\_\_\_

Firm Name: \_\_\_\_\_

Address: \_\_\_\_\_

# REVISED BID FORMS C-3

## Buy America Certification (Title 49 U.S.C. Section 50101)

<b>PROJECT NAME:</b>	<b>Rehabilitate Airfield Signage &amp; Lighting</b>
<b>AIRPORT NAME:</b>	<b>Roanoke-Blacksburg Regional Airport</b>
<b>AIP NUMBER:</b>	<b>3-51-0045-062-2019 (Design)</b>

This solicitation and any resulting contract are subject to the Buy America requirements of 49 U.S.C. Section 50101. The bidder certifies it and all associated subcontractors will comply with the Buy American preferences established under Title 49 U.S.C. Section 50101 as follows:

### **U.S.C. Section 50101 - Buying goods produced in the United States**

- (a) Preference. - The Secretary of Transportation may obligate an amount that may be appropriated to carry out section 106(k), 44502(a)(2), or 44509, subchapter I of chapter 471 (except section 47127), or chapter 481 (except sections 48102(e), 48106, 48107, and 48110) of this title for a project only if steel and manufactured goods used in the project are produced in the United States.
- (b) Waiver. - The Secretary may waive subsection (a) of this section if the Secretary finds that -
  - (1) Applying subsection (a) would be inconsistent with the public interest;
  - (2) The steel and goods produced in the United States are not produced in a sufficient and reasonably available amount or are not of a satisfactory quality;
  - (3) When procuring a facility or equipment under section 44502(a)(2) or 44509, subchapter I of chapter 471 (except section 47127), or chapter 481 (except sections 48102(e), 48106, 48107, and 48110) of this title -
    - A. The cost of components and subcomponents produced in the United States is more than 60 percent of the cost of all components of the facility or equipment; and
    - B. Final assembly of the facility or equipment has occurred in the United States; or
  - (4) Including domestic material will increase the cost of the overall project by more than 25 percent.
- (c) Labor Costs. - In this section, labor costs involved in final assembly are not included in calculating the cost of components.

\* \* \* \* \*

As a matter of bid responsiveness, the bidder or offeror must complete and submit this certification with their bid proposal. The bidder must sign and date the certification. The bidder/offeror must indicate how they propose to comply with the Buy America provision by selecting one of the following certification statements.

## REVISED BID FORMS C-3

- The bidder hereby certifies that it will comply with Title 49 U.S.C Section 50101(a) by only installing steel and manufactured products produced in the United States of America. The bidder further agrees that if chosen as the apparent low bid, it will submit documentation to the owner that demonstrate all steel and manufactured products are 100% manufactured in the United States.
  
- The bidder hereby certifies that it cannot fully comply with the Buy America preferences of Title 49 U.S.C Section 50101(a); the bidder therefore requests a waiver per Title 49 U.S.C Section 50101(b). The bidder further agrees that upon notification from the Owner, the bidder identified with the apparent low bid agrees to prepare and submit waiver request and component calculation information to the owner within seven (7) calendar days of the date of the notice of apparent low bid.

**Bidder's Firm Name:** \_\_\_\_\_

**Date:** \_\_\_\_\_

**Signature:** \_\_\_\_\_

# REVISED BID FORMS C-3

## Compliance with state law; foreign and domestic businesses authorized to transact business in the Commonwealth

Pursuant to Virginia Code Section 2.2-4311.2 (effective July 1, 2010), each bidder or offeror organized or authorized to transact business in the Commonwealth of Virginia pursuant to Title 13.1 or Title 50 of the Code of Virginia, (1950), as amended, or as otherwise required by law, is required to include in its bid or proposal its Virginia State Corporation Commission (SCC) Identification Number. Any bidder or offeror that is not required to be authorized to transact business in the Commonwealth of Virginia as a domestic or foreign business entity under Title 13.1 or Title 50 or as otherwise required by law is required to include in its bid or proposal a statement describing why the bidder or offeror is not required to be so authorized.

Please complete the following:

A. \_\_\_\_\_ Bidder/Offeror is a Virginia business entity organized and authorized to transact business in Virginia and such bidder's/offeror's SCC Identification Number is:

\_\_\_\_\_.

B. \_\_\_\_\_ Bidder/Offeror is an out-of-state (foreign) business entity authorized to transact business in Virginia and such bidder's/offeror's SCC Identification Number is:

\_\_\_\_\_.

C. \_\_\_\_\_ Bidder/Offeror does not have an Identification Number issued to it by the SCC and such bidder/offeror is not required to be authorized to transact business in Virginia by the SCC for the following reason(s):

\_\_\_\_\_.

\_\_\_\_\_.

Please attach additional sheets of paper if more space is needed to explain why such bidder/offeror is not required to be authorized to transact business in Virginia)

NAME OF BIDDER \_\_\_\_\_

ADDRESS \_\_\_\_\_

SIGNATURE \_\_\_\_\_

NAME (TYPE OR PRINT) \_\_\_\_\_

OFFICIAL TITLE \_\_\_\_\_

DATE \_\_\_\_\_

TELEPHONE NO. ( \_\_\_\_\_ ) \_\_\_\_\_

"REGISTERED VIRGINIA CONTRACTOR" NO. \_\_\_\_\_



# REVISED BID FORMS C-3

ESCROW ACCOUNT REQUESTED (if applicable): YES \_\_\_\_\_ NO \_\_\_\_\_

The above party agrees, if use of the escrow account procedure is desired, to submit an executed escrow agreement to the Commission within fifteen (15) calendar days after notification of the award of bid by the Commission. Failure to submit the executed escrow agreement form within such fifteen-day period shall result in a forfeiture of the right to the use of such escrow account procedure. See Section 11.4 and 15.5 of Instructions to Bidders.

The undersigned hereby acknowledges the receipt of the following Addenda to the Contract Documents.

Addendum No. One Issued \_\_\_\_\_ (DATE)  
Addendum No. Two Issued \_\_\_\_\_ (DATE)  
Addendum No. Three Issued \_\_\_\_\_ (DATE)  
Addendum No. Four Issued \_\_\_\_\_ (DATE)  
Addendum No. Five Issued \_\_\_\_\_ (DATE)

**EACH BIDDER MUST COMPLETE AND SIGN THE INFORMATION BLOCK BELOW OR ELSE ITS BID SHALL BE DETERMINED TO BE NON-RESPONSIVE.**

\_\_\_\_\_  
Complete Firm Name of Bidder

\_\_\_\_\_  
Signature of Authorized Official

\_\_\_\_\_  
Name & Title of Signing Official

Business Address: \_\_\_\_\_  
\_\_\_\_\_

Telephone: ( ) \_\_\_\_\_ FAX: ( ) \_\_\_\_\_  
Area Code Area Code

EMAIL: \_\_\_\_\_ CELL: \_\_\_\_\_

CONTRACTOR'S VIRGINIA "CLASS A" CONTRACTOR NO: \_\_\_\_\_

# REVISED BID FORMS C-3



**Roanoke Regional Airport Commission**  
**ATTN: Accounts Payable**  
**5202 Aviation Drive NW**  
**Roanoke VA 24012**  
**540/362-1999**  
[finance@flyroa.com](mailto:finance@flyroa.com)

**Roanoke Regional Airport Commission** pays its vendors electronically rather than by paper check. Your payments will be deposited into the checking account of your choice. In addition to having the money deposited electronically, you will also be notified of the deposit by e-mail. The e-mail will provide you with all the information that would normally be on your check stub. To receive payments electronically, you must print, complete this form, include a voided check and return both to the **address or e-mail** above. By signing below, you authorize Roanoke Regional Airport Commission to initiate credit entries, and if necessary, debit entries and adjustments for any credit entries in error to your checking account.

Payee Information	
Payee Name:	SSN or Federal ID #:
Remit Address(es) for applicable accounts:	

Bank Information	
Bank Name:	
Name on Account:	
Account #:	
Routing #:	
Email Address:	

The E-MAIL ADDRESS is for payment notification.

Name(s): \_\_\_\_\_ Title: \_\_\_\_\_  
*Please print*

\_\_\_\_\_  
**Authorized Signature** **Date**

**PLEASE INCLUDE VOIDED CHECK**

# REVISED BID FORMS C-3

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